

Title	Planning Applications
To:	Planning Control Committee
On:	23 May 2017
By:	Development Manager
Status:	For Publication

Executive Summary

The attached reports present members with a description of various planning applications, the results of consultations, relevant policies, site history and issues involved.

My recommendations in each case are given in the attached reports.

This report has the following implications

Township Forum/ Ward: Identified in each case.

Policy: Identified in each case.

Resources: Not generally applicable.

Equality Act 2010: All planning applications are considered in light of the Equality Act 2010 and associated Public Sector Equality Duty, where the Council is required to have due regard for:
The elimination of discrimination, harassment and victimisation;
The advancement of equality of opportunity between persons who share a relevant protected characteristic and person who do not share it;
The fostering of good relations between persons who share a relevant protected characteristic and person who do not share it; which applies to people from the protected equality groups.

Human Rights: All planning applications are considered against the provisions of the Human Rights Act 1998.

Under Article 6 the applicants (and those third parties who have made representations) have the right to a fair hearing and to this end full consideration will be given to their comments.

Article 8 and Protocol 1 of the First Article confer a right to respect private and family life and a right to the protection of property, ie peaceful enjoyment of one's possessions which could include a person's home, and other land and business assets.

In taking account of the Council policy as set out in the Bury Unitary Development Plan 1997 and all material planning considerations, I have concluded on balance that the rights conferred upon the applicant/ objectors/ residents/ other interested party by Article 8 and Article 1 of the First Protocol may be interfered with, since such interference is in accordance with the law and is justified in the public interest. Any restriction of these rights posed by refusal/ approval of the application is legitimate since it is proportionate to the wider benefits of such a decision, is based

upon the merits of the proposal, and falls within the margin of discretion afforded to the Council under the Town & Country Planning Acts.

The Crime and Disorder Act 1998 imposes (without prejudice to any other obligation imposed on it) a duty upon the Council to exercise its functions and have due regard to the likely effect of the exercise of its functions on, and the need to do all that it reasonably can to prevent crime and disorder in its area. In so doing and on making planning decisions under the Town and Country Planning Acts, the Planning Control Committee shall have due regard to the provisions of the Crime and Disorder Act 1998 and its implications in the exercise of its functions.

Development Manager

Background Documents

1. The planning application forms and plans submitted therewith.
2. Certificates relating to the ownership.
3. Letters and Documents from objectors or other interested parties.
4. Responses from Consultees.

FOR FURTHER INFORMATION ON THE CONTENTS OF EACH REPORT PLEASE CONTACT INDIVIDUAL CASE OFFICERS IDENTIFIED IN EACH CASE.

01	Township Forum - Ward: Radcliffe - East	App No. 59984
	Location: 77 Bury Road, Radcliffe, Manchester, M26 2UT	
	Proposal: Outline - Demolition of existing care home and erection of 13 no. dwellings	
	Recommendation: Approve with Conditions	Site Visit: N
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02	Township Forum - Ward: Prestwich - Sedgley	App No. 60045
	Location: Texaco Petrol Station, Bury Old Road, Prestwich, Manchester, M25 0EY	
	Proposal: Demolition of existing structures and erection of an retail (A1) unit and synagogue with associated car parking, landscaping and access arrangement	
	Recommendation: Minded to Approve	Site Visit: N
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03	Township Forum - Ward: Bury East	App No. 61163
	Location: Land at Shaw Street/Gladstone Street, Bury, BL9 7QD	
	Proposal: Erection of 3 no. dwellings	
	Recommendation: Approve with Conditions	Site Visit: N
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04	Township Forum - Ward: Ramsbottom and Tottington - Ramsbottom	App No. 61223
	Location: Sheep Hey Farm House, Leaches Road, Shuttleworth, Ramsbottom, Bury, BL0 0ND	
	Proposal: Retention of shed to store hay, feed and associated equipment; retention of boundary fencing	
	Recommendation: Approve with Conditions	Site Visit: N
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05	Township Forum - Ward: Bury West - Church	App No. 61286
	Location: Land at 4 Mile Lane, Bury, BL8 2DS	
	Proposal: Erection of 1 no. dwelling at side	
	Recommendation: Approve with Conditions	Site Visit: N
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06	Township Forum - Ward: Prestwich - Sedgley	App No. 61296
	Location: 130 Bury New Road, Prestwich, Manchester, M25 0AA	
	Proposal: Two storey side extension to provide additional retail space (Class A1) at ground floor level and office space (Class B1a) at first floor level with associated car park alterations and landscaping works	
	Recommendation: Approve with Conditions	Site Visit: N

07	Township Forum - Ward: Ramsbottom and Tottington - Ramsbottom	App No. 61299
	Location: Site of former police station, Bridge Street, Ramsbottom, Bury, BL0 9AB	
	Proposal: Creation of car park for temporary period of 3 years	
	Recommendation:	Site Visit: N
08	Township Forum - Ward: Bury East	App No. 61300
	Location: 60 Bolton Street, Bury, BL9 0LL	
	Proposal: Change of use from solicitors office (Class A2) to private hire taxi booking office (Sui Generis)	
	Recommendation: Approve with Conditions	Site Visit: N
09	Township Forum - Ward: Whitefield + Unsworth - Besses	App No. 61320
	Location: 17 Beverley Close, Whitefield, Manchester, M45 8BB	
	Proposal: Single storey rear extension	
	Recommendation: Approve with Conditions	Site Visit: N
10	Township Forum - Ward: Bury East - Redvales	App No. 61369
	Location: Land adjacent to 23 Meadway, Bury, BL9 9TY	
	Proposal: Outline application for 2 no. detached dwellings with detached double garages with details of access.	
	Recommendation: Approve with Conditions	Site Visit: N

Ward: Radcliffe - East

Item 01

Applicant: Mr Ikhtlaq Gorski

Location: 77 Bury Road, Radcliffe, Manchester, M26 2UT

Proposal: Outline - Demolition of existing care home and erection of 13 no. dwellings

Application Ref: 59984/Outline Planning
Permission

Target Date: 05/01/2017

Recommendation: Approve with Conditions

Description

The application site comprises a level site some 0.2ha in area, with vehicular and pedestrian access off Orkney Close. There is currently a vacant 3 storey late Victorian/Edwardian former nursing home building on the site constructed in red brickwork and slate roof. The site, although flat, is slightly elevated to Bury Road and is bounded along this frontage by a low brick wall. The remaining boundary features comprise timber fencing of 2 metres in height. There are 2 trees on site, which are protected by a Tree Preservation Order (TPO) and these are on the boundaries with Bury Road and Orkney Close.

There are residential properties to the north, south and west and the Metrolink line is located to the east of the site.

The applicant seeks outline permission for the demolition of the existing care home building and the erection of 13 dwellings and includes with the means of access, layout and scale at this stage.

The proposed dwellings would be located in two terraces - one along the frontage with Bury Road and a second at 90 degrees to the Metrolink line. The proposed dwellings would be two storeys in height with additional living accommodation in the roof space. The proposed elevation plans indicate a dormer but it should be noted that the appearance of the dwellings would be dealt with at reserved matters stage.

The proposed development would be accessed from Orkney Close with pedestrian accessed from Bury Road for plots 1 - 6.

Relevant Planning History

31647 - Extension to existing care home to provide 36 bedrooms and car parking at 77 Bury Road, Radcliffe. Approved - 13 July 1995

32898 - Demolition of existing Care home and the erection of 42 bed care home at 77 Bury Road, Radcliffe. Approved - 15 April 1997

46351 - Demolition of existing care home and erection of new 42 bed care home with ancillary accommodation and parking at 77 Bury Road, Radcliffe. Withdrawn - 14 July 2006

50227 - Outline planning application for the erection of 42 bed care home and managers apartment. Details include layout of site and access at 77 Bury Road, Radcliffe. Withdrawn - 9 October 2008.

50596 - Outline planning permission for the erection of two/three storey care home (resubmission) at 77 Bury Road, Radcliffe. Approved with conditions - 24 December 2008.

50735 - Outline planning application for two/three storey block of 14 no. apartments at 77

Bury Road, Radcliffe. Approved with conditions - 19 February 2009.

52455 - Change of use from former residential care home to a single house (Use Class C3) at 77 Bury Road, Radcliffe. Approved with conditions - 1 June 2010.

Publicity

The neighbouring properties were notified by means of a letter on 21 October and a press notice was published in the Bury Times on 27 October 2016. Site notices were posted on 2 November 2016.

1 letter of support has been received from the occupiers of 81 Bury Road, which has raised the following issues:

- Like to strongly support the application for 13 houses as the current building is an eyesore and devalues houses in the area.
- It attracts litter and crime.
- The plans are well thought out and will not overlook existing properties.
- The design would provide a safe close for young children to play out in.
- There would be sufficient parking available.
- The proposed houses are 3 bed dwellings that would provide housing for young professionals and working families.

3 letters have been received from the occupiers of 19 Orkney Close, which have raised the following issues:

- Generally in favour of the development, but have concerns regarding parking.
- The narrow entrance to Orkney Close is often lined either side with parked cars and it can be difficult to get past to the top end of Orkney Close.
- Double yellow lines would not help as would force cars to park further up the street.
- Concerned about access for vehicles during construction.
- Overwhelmed by the number of dwellings proposed and feel that 13 is a lot given the space available.
- Concerned that the number of parked vehicles would impact upon pedestrian footpaths.

The respondents have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objections, subject to the inclusion of conditions relating to highway improvements, construction traffic management plan, measures to prevent mud from passing onto the highway and car parking.

Drainage Section - Comments awaited.

Environmental Health - Contaminated Land - No objections, subject to the inclusion of conditions relating to contaminated land and remediation.

Design for security - Crime impact statement should be submitted.

United Utilities - No objections, subject to the inclusion of conditions relating to foul and surface water drainage.

Metrolink - Require further information to be able to assess the impact upon metrolink line. This has been submitted and further comments will be reported in the Supplementary Report.

Recommend conditions relating to stability of metrolink embankment, construction management plan and noise.

The Coal Authority - Raise a concern. Further information has been submitted and comments will be provided in the Supplementary Report.

GM Ecology Unit - No objections, subject to the inclusion of conditions relating to bats, nesting birds and a landscape management plan and a informative relating to invasive species.

Transport for Greater Manchester - No objections, subject to the inclusion of a condition relating to the bus stop.

Waste Management - Comments awaited.

Unitary Development Plan and Policies

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H2/6	Garden and Backland Development
EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN2	Conservation and Listed Buildings
EN3/1	Impact of Development on Archaeological Sites
EN3/2	Development Affecting Archaeological Sites
EN3/3	Ancient Monuments
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/2	Noise Pollution
EN7/5	Waste Water Management
EN8	Woodland and Trees
EN8/1	Tree Preservation Orders
HT2/4	Car Parking and New Development
HT5/1	Access For Those with Special Needs
HT6/2	Pedestrian/Vehicular Conflict
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle (residential) - Following revocation of the North West Regional Strategy on 20 May 2013, there is no statutory housing target for Bury. Work has commenced on the Greater Manchester Spatial Framework and this will bring forward a new statutory housing target for the Borough. This will subsequently be incorporated into Bury's future Local Plan.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable housing land.

Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

The site would be located within the urban boundary and would be surrounded by residential properties. As such, the proposed development would not conflict with the surrounding land uses and would be in a sustainable location with good access to public transport and services. The site contains a detached building and is previously developed land. As such, the proposed development would be acceptable in principle and would be in accordance with Policy H1/2 of the Bury Unitary Development Plan and the NPPF.

Heritage - The building, which was last used as a nursing home, dates back to the late Victorian/Edwardian and is slightly elevated to Bury Road. The site was included within Withins Lane Colliery in 1893 and by 1907, the railway sidings had been removed and the site redeveloped to provide a single building, which was in use as a rectory. Given its architectural style and age, it is considered that the existing building would be a non-designated heritage asset.

Paragraph 135 of the NPPF states that the effect of an application on a non designated heritage asset should be taken into account when determining an application, with a balance judgement to any harm or loss and the significance of a heritage asset.

The style of the building is modest with no over embellishment. The building has been used for a number of uses over the years - rectory, offices and a nursing home, which has removed the historical interest internally. A fire escape was added to the rear, which is constructed from corrugated metal, which significantly detracts from the character of the building.

The last use of the building was as a care home in 2007 and the building has been vacant ever since. Various planning applications were submitted between 2006 and 2010 on the site, which confirmed that the building was in a poor state of repair. The various consents were not implemented and the fabric of the building has worsened over the past 10 years and the building is dilapidated and un-economic to repair. As such, it is considered that the works undertaken over the years and the dilapidation of the building have reduced the degree of significance and the demolition of the building and redevelopment of the site would be acceptable in this case. Therefore, the proposed development would be in accordance with Policy EN2 of the Bury Unitary Development Plan and the NPPF.

Design and layout - The proposed dwellings would be two storeys in height with dormers and additional living accommodation in the roofspace. The existing dwellings are two storeys in height and there is a mix of types and design. As such, the proposed dwellings would not be a prominent feature or appear out of scale. Whilst the proposed dormers are considered to be too large, this would be a consideration for the external appearance as part of the reserved matters application. The agent has been made aware of these concerns.

The level of private amenity space would be acceptable and there would be space within the rear gardens for bin storage. Two areas for use as a bin collection point would be provided for use on collection days. Therefore, the proposed development would be in accordance with Policies EN1/2, H2/1 and H2/2 of the Bury Unitary Development Plan.

Trees - There are two trees on the site, which are protected by a Tree preservation Order (TPO). The trees are located on the boundary of the site with Bury Road and Orkney Close respectively. The proposed site plan indicates that both trees would be retained and tree protection measures would be secured by a condition. Therefore, the proposed development would not have an adverse impact upon the protected trees and the character of the area and would be in accordance with Policy EN8/2 of the Bury Unitary Development Plan.

Bats - A bat survey was submitted as part of the application and a dusk survey had been undertaken. Whilst there was no bat emergence at 77 Bury Road, commuting and foraging activity was recorded during dusk observation. As such, it is apparent that there is a roost located in relatively close proximity to the building. Therefore, it is recommended that all roof slates are removed in a careful and controlled manner prior to the demolition works. GM Ecology Unit has no objections to the proposal, subject to the inclusion of conditions relating to bats, nesting birds and a landscape management plan and an informative relating to invasive species. Therefore, the proposed development would not cause harm to a protected species and would be in accordance with Policies EN6 and EN6/3 of the Bury Unitary Development Plan.

Noise - The site is located adjacent to the Metrolink line. The layout of the development would be secured at this stage and the proposed site plan indicates that the proposed dwellings would be 8.2 metres (gable elevation) and 62 metres (rear elevation) from the metrolink line respectively. This would be no closer than the existing dwellings on Olsberg Close. Pollution Control have no objections to the proposal, subject to the inclusion of a condition relating to a noise survey and the provision of appropriate acoustic insulation and ventilation. It is considered that the proposed noise survey should be provided at the first reserved matters stage, when the detailed design and layout of the proposed dwellings would be assessed. Therefore, the proposed development would not have a significantly adverse impact upon the amenity of the future occupiers and would be in accordance with Policy EN7/2 of the Bury Unitary Development Plan and the NPPF.

Highways issues - The proposed development would be accessed from Orkney Close and appropriate levels of visibility would be provided. The proposed development would provide a footpath along the Orkney Street boundary, which is welcomed. The proposed site plan indicates two bin collection points and the refuse vehicle would collect from Orkney Close and turning facilities would be provided. The Traffic Section has no objections, subject to the inclusion of conditions relating to highway improvements, construction traffic management plan, measures to prevent mud from passing onto the highway and car parking. Therefore, the proposed development would not be detrimental to highway safety and would be in accordance with Policies EN1/2, H2/1 and H2/2 of the Bury Unitary Development Plan.

The Metrolink line forms the boundary to the north west of the site and Transport for Greater Manchester (TfGM) has requested additional information to be able to assess the impact upon the Metrolink line. This information has been submitted and further comments will be reported in the Supplementary Report.

Parking - SPD11 states that the maximum parking standards is 2 spaces per 3 bed dwelling. This equates to 26 parking spaces.

The proposed site plan indicates that 22 spaces would be provided. Given that the site has good access to public transport, it is considered that the level of parking provision would be acceptable in this instance. Therefore, the proposed development would be in accordance with Policy HT2/4 of the Bury Unitary Development Plan and SPD11.

Planning Obligations - A contribution for recreation provision would be required in accordance with Policy RT2/2 of the Bury Unitary Development Plan and SPD1 and this would be secured by a condition at outline stage.

Response to objectors

- The issues relating to parking and layout have been addressed in the main report.
- The issues relating to parking and access during construction would be addressed by a condition relating to a construction traffic management plan.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during pre-application discussions to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. Applications for approval of reserved matters must be made not later than:
 - the expiration of three years beginning with the date of the grant of outline planning permission; and
 - that the development to which the permission relates must be begun not later than the expiration of two years from the final approval of the reserved matters or, in the case of approval on different dates, the final approval of the last such matter to be approved.

Reason. Required to be imposed pursuant to Section 51 of the Planning and Compulsory Purchase Act 2004.

2. Before the development is commenced, the applicant shall submit detailed plans and particulars to the Local Planning Authority, and obtain their approval under the Town and Country Planning Acts, of the following reserved matters; the appearance and the landscaping of the site.

Reason. To ensure the satisfactory development of the site and because this application is in outline only.

3. This decision relates to drawings numbered IG1-00, IG1-01, IG1-02 B, IG1-03 A and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

4. All trees to be retained on site shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are implemented and all measures required shall remain in situ until the development has been completed.

Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

5. No trees subject to a Tree Preservation Order shall be felled, lopped or topped before, during or after the construction period.

Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

6. The demolition works hereby approved shall be carried out in accordance with the recommendations contained in Section 10 of the Surveys in Relation to Bats and Breeding Birds, by The Tyrer Partnership, dated October 2015. If the demolition hereby approved does not commence before 30 April 2018, the building must be re-assessed for bat roosting potential and the results supplied to and approved in writing by the Local Planning Authority.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.

7. No works to trees or shrubs shall occur or demolition commence between 1st March and 31st August inclusive in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and has been submitted to and approved in writing by the Local Planning Authority.

Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and National

Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

8. At first reserved matters stage, a landscaping management scheme shall be submitted and shall include elements to mitigate for the loss of trees, shrubs and nesting bird habitat. The development shall be carried out in accordance with the approved details.
Reason. To secure the satisfactory development of the site and in the interests of visual amenity and ecology pursuant to Policies EN1/2 - Townscape and Built Design, EN8/2 – Woodland and Tree Planting and EN6 - Conservation of the Natural Environment of the Bury Unitary Development Plan and Section 11 of the National Planning Policy Framework.
9. At the first reserved matters application, a noise survey shall be submitted to the Local Planning Authority. If the ambient noise in any habitable room of any dwelling exceeds 35dB for living rooms and 30dB for bedrooms, a scheme for acoustic insulation and independent ventilation scheme requirements should be submitted, approved and implemented in accordance with the approved scheme and be in situ prior to first occupation.
Reason. To protect the amenity of the occupants of the premises once the development hereby approved is occupied pursuant to Policy EN7/2 - Noise Pollution of the Bury Unitary Development Plan.
10. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
11. Following the provisions of Condition 10 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
12. Following the provisions of Condition 10 of this planning permission, where ground gas remediation / protection measures are required, the approved Remediation Strategy must be carried out to the written satisfaction of the Local Planning Authority within approved timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To alleviate any possible risk associated with the production of landfill gas and ground gas in accordance with the recommendations of the Environment Agency and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

13. As the development comprises 13 units, the development authorised by this permission shall not begin unless and until the Local Planning Authority has approved in writing a scheme to secure recreation provision, which shall include a mechanism for delivery, in accordance with policy RT2/2 – Recreation Provision in New Housing Development and its associated SPD1 – Open Space, Sport and Recreation Provision in New Housing development. The scheme shall be submitted as part of the reserved matters application and the recreation provision shall be delivered in full accordance with the approved details.

Reason - To ensure that the development would contribute to satisfying the need for recreation provision pursuant to Bury Unitary Development Plan Policy RT2/2 - Recreation Provision In New Residential Development and the associated Supplementary Planning Document 1 - Open Space, Sport and Recreation in New Housing Development.

14. Notwithstanding the details indicated on approved plan reference IG1-02 Revision B, full details of the following highway aspects shall be submitted at first reserved matters application stage:

- Topographical survey of the site and surrounding adopted highways to confirm the correct limits of adoption;
- Formation of the proposed means of access to the site from Orkney Close to a specification to be agreed, incorporating the reinstatement of the redundant access, widening of the block paved carriageway to a minimum of 5.5m in width across the proposed junction, widening of the northerly footway abutting the site to a minimum of 1.8m in width and all necessary highway and highway drainage remedial works;
- Formation of pedestrian accesses to the plots fronting Bury Road including all alterations required to the existing boundary treatment and all necessary highway remedial works;
- Provision of visibility splays at the proposed junction with Orkney Close in accordance with the standards in Manual for Streets;
- Adequate in-curtilage bin storage arrangements at each plot and refuse collection facilities at the interface with the adopted highway for use on collection day, to be agreed with Waste Management;
- Proposed internal private road/footway layout to the appropriate standards in Manual for Streets;
- Adequate turning facilities within the curtilage of the site with associated swept path analysis;
- Provision of a street lighting scheme for the proposed residential development and the junction of the proposed access with Orkney Close and proposals to improve existing lighting levels if required as a result of the assessment.

The details subsequently approved shall be implemented to an agreed programme.

Reason. To secure the satisfactory development of the site in terms of highway safety, ensure good highway design and maintain the integrity of the adopted highway, in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy H2/1 - The Form of New Residential Development

Policy H2/2 - The Layout of New Residential Development

Policy EN1/2 - Townscape and Built Design.

15. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and approved in writing with the Local Planning Authority and shall confirm/provide the following:
- Access route for demolition/construction traffic from the highway network;
 - Hours of operation and number of vehicle movements;
 - Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
 - Parking on site or on land within the applicant's control of operatives' and demolition/construction vehicles together with storage on site of demolition/construction materials.

The approved plan shall be adhered to throughout the demolition/construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the demolition/construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of demolition/construction materials.

Reason. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety pursuant to the following Policies of the Bury Unitary Development Plan:

Policy H2/1 - The Form of New Residential Development

Policy H2/2 - The Layout of New Residential Development

Policy EN1/2 - Townscape and Built Design.

16. Before the development is commenced, details shall be submitted to and approved by the Local Planning Authority to cover measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations. The approved details shall be implemented and maintained thereafter during the period of construction.

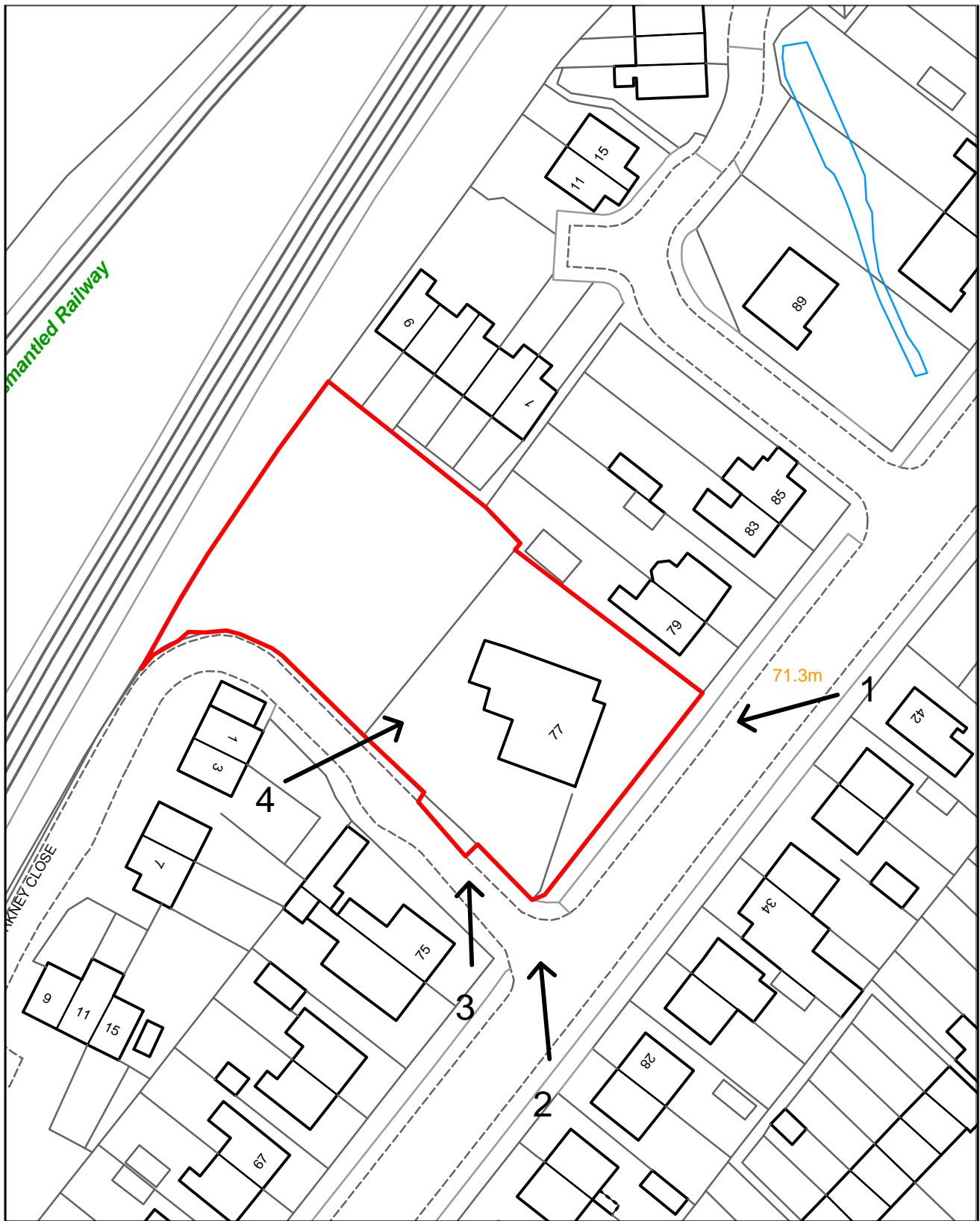
Reason. - To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Policy H2/2 - The Layout of New Residential Development of the Bury Unitary Development Plan.

17. The car parking indicated on approved plan reference IG1-02 Revision B shall be surfaced, demarcated and made available for use prior to the development hereby approved being occupied and thereafter maintained at all times.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to Policy HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 59984

**ADDRESS: 77 Bury Road
Radcliffe**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

59984

Photo 1



Photo 2

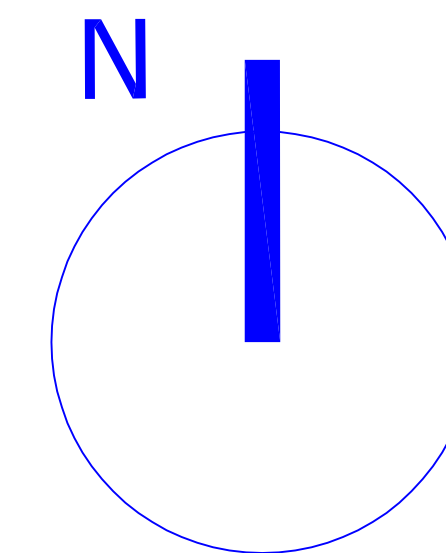
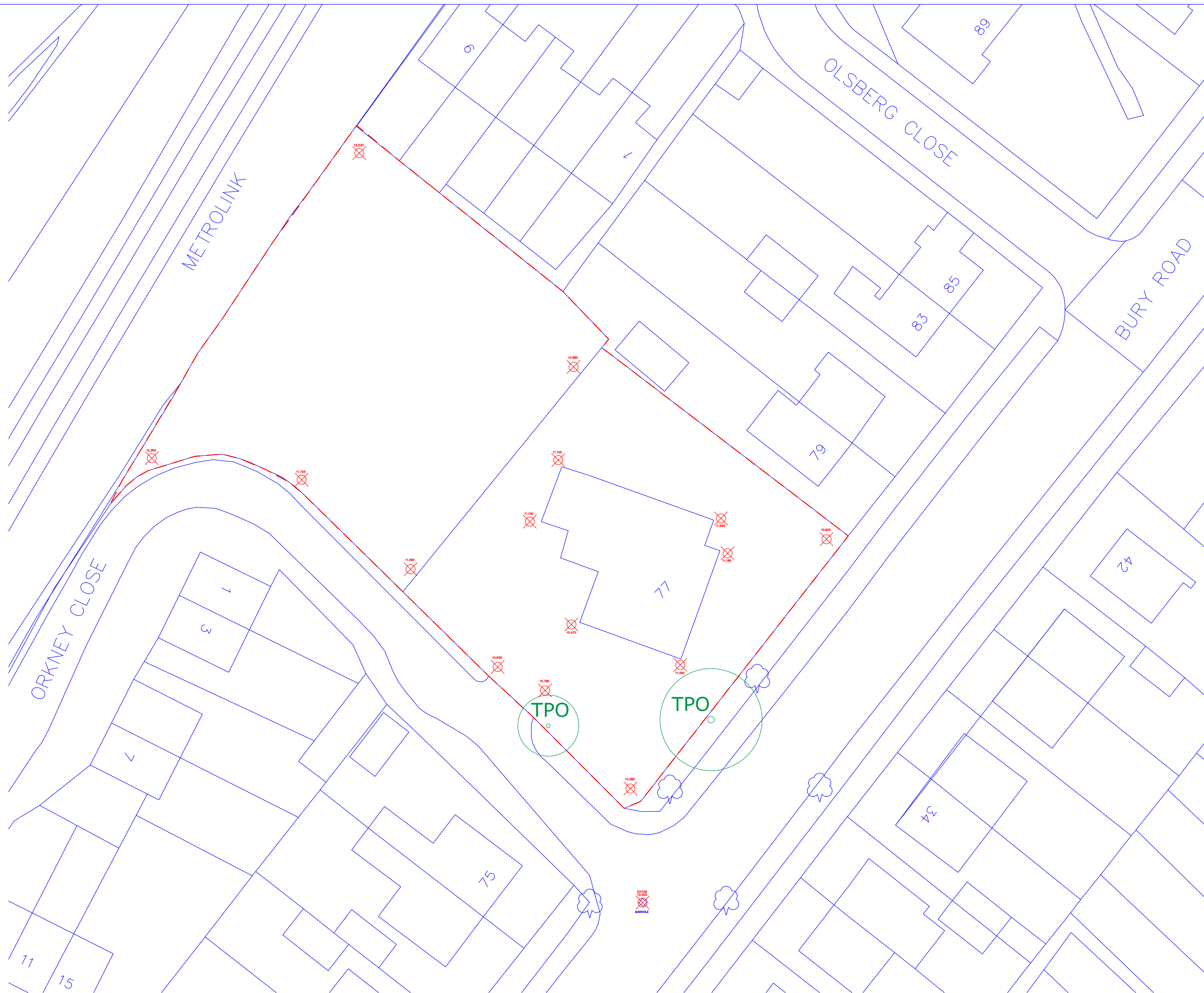


Photo 3



Photo 4





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Land at 77 Bury Road
Raddcliffe
M26 2UT

scale 1:200@A1
April 2016

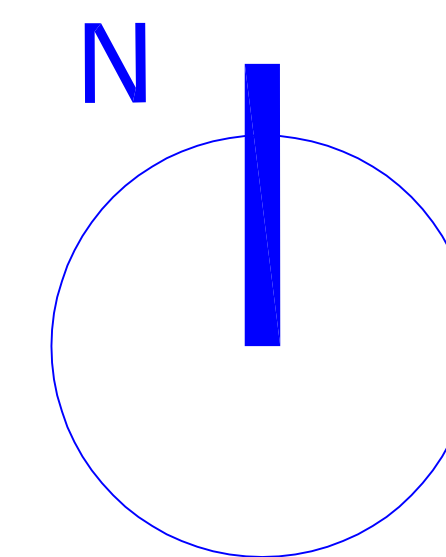
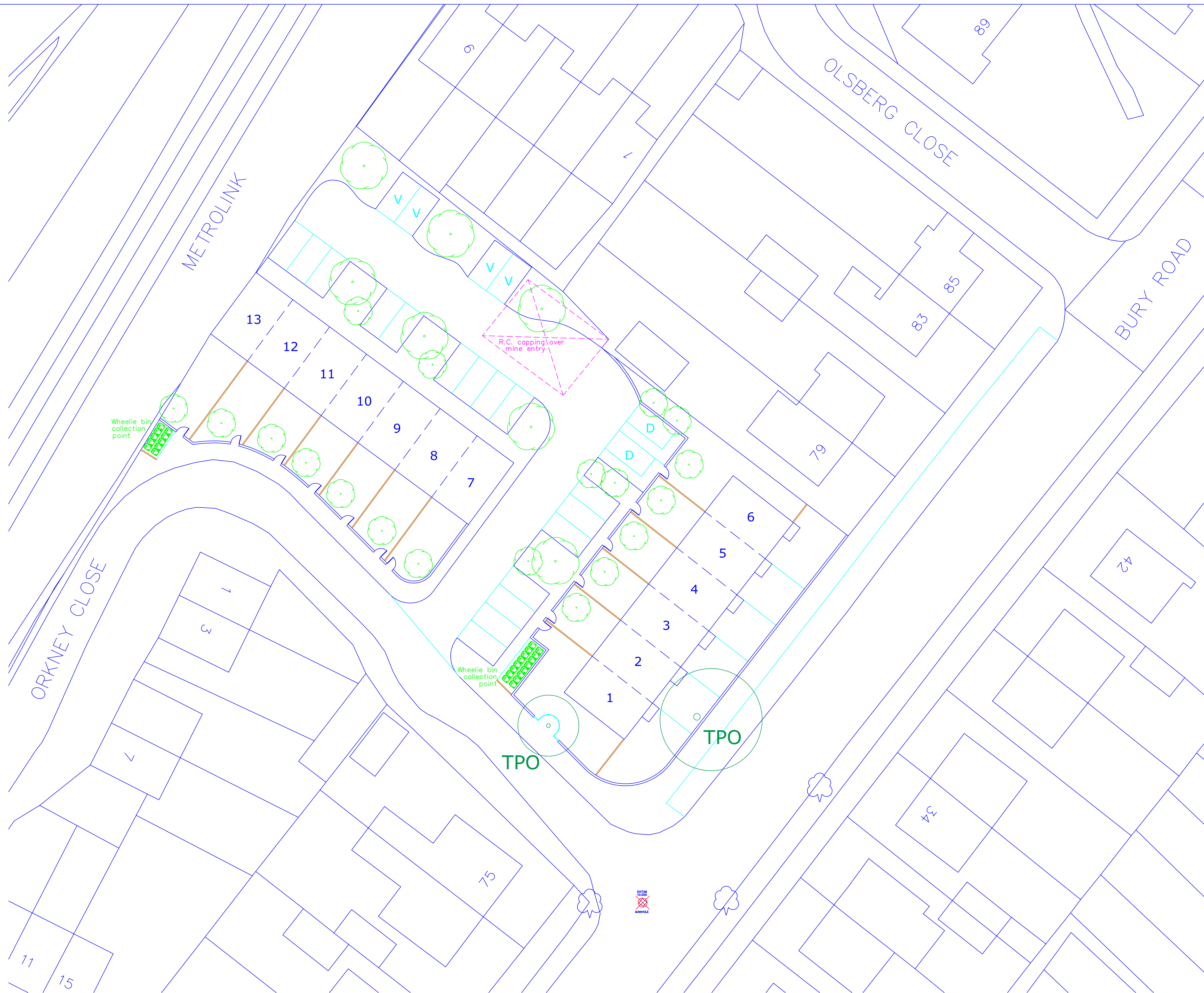
IG1-01 Existing Site Layout
showing levels

PPY design Ltd

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EXISTING SITE LAYOUT

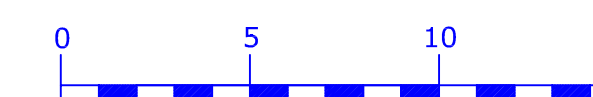


Schedule:

13no. 3 bed dwellings
on 2 storeys plus within loft
utilising dormers

B Layout amended 02.02.2017
A Historic mine entry indicated 15.12.2016

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Land at 77 Bury Road
Raddiffe
M26 2UT
scale 1:200@A1
April 2016

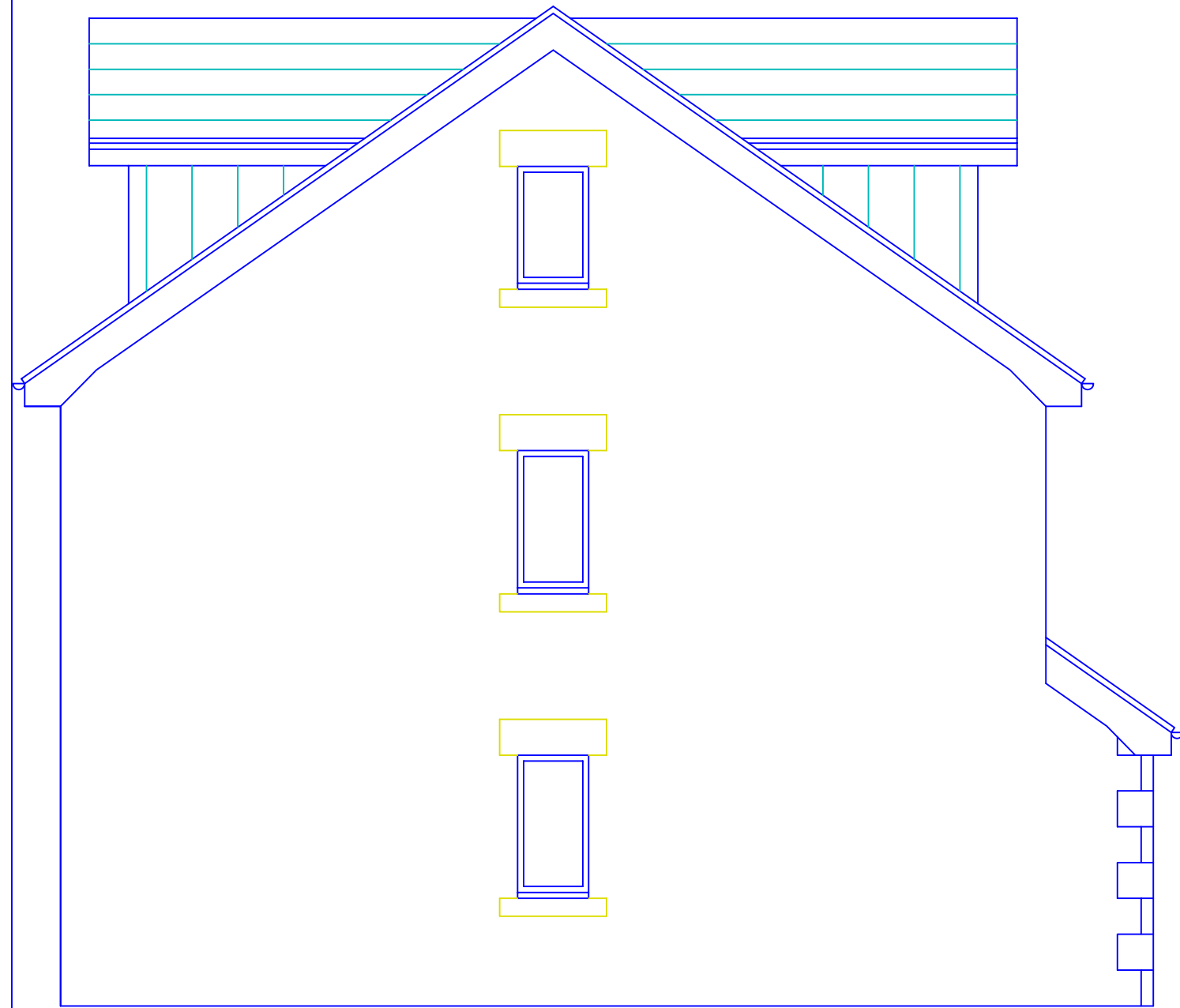
IG1-02B Proposed Site Layout

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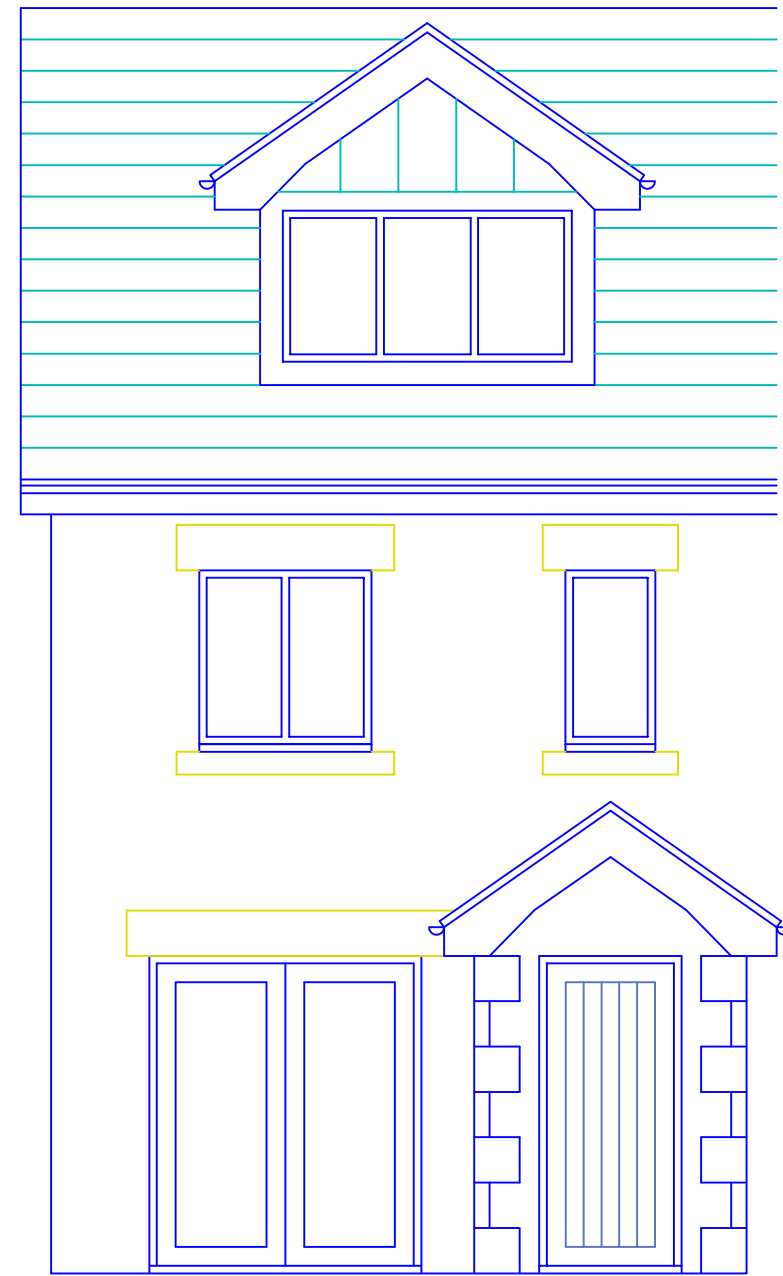
2 Helmshore Road, Holcombe Village
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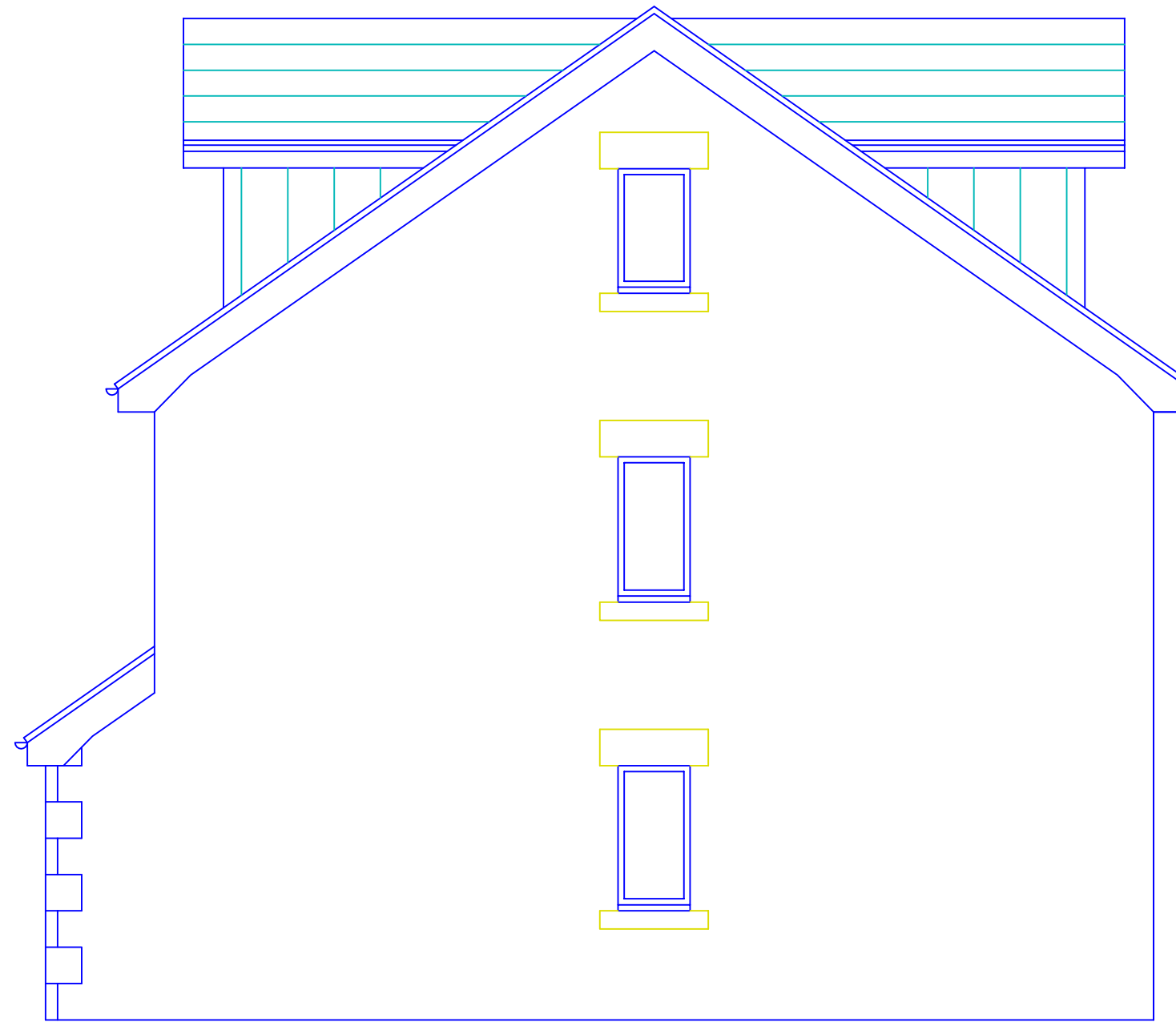
PROPOSED SITE LAYOUT



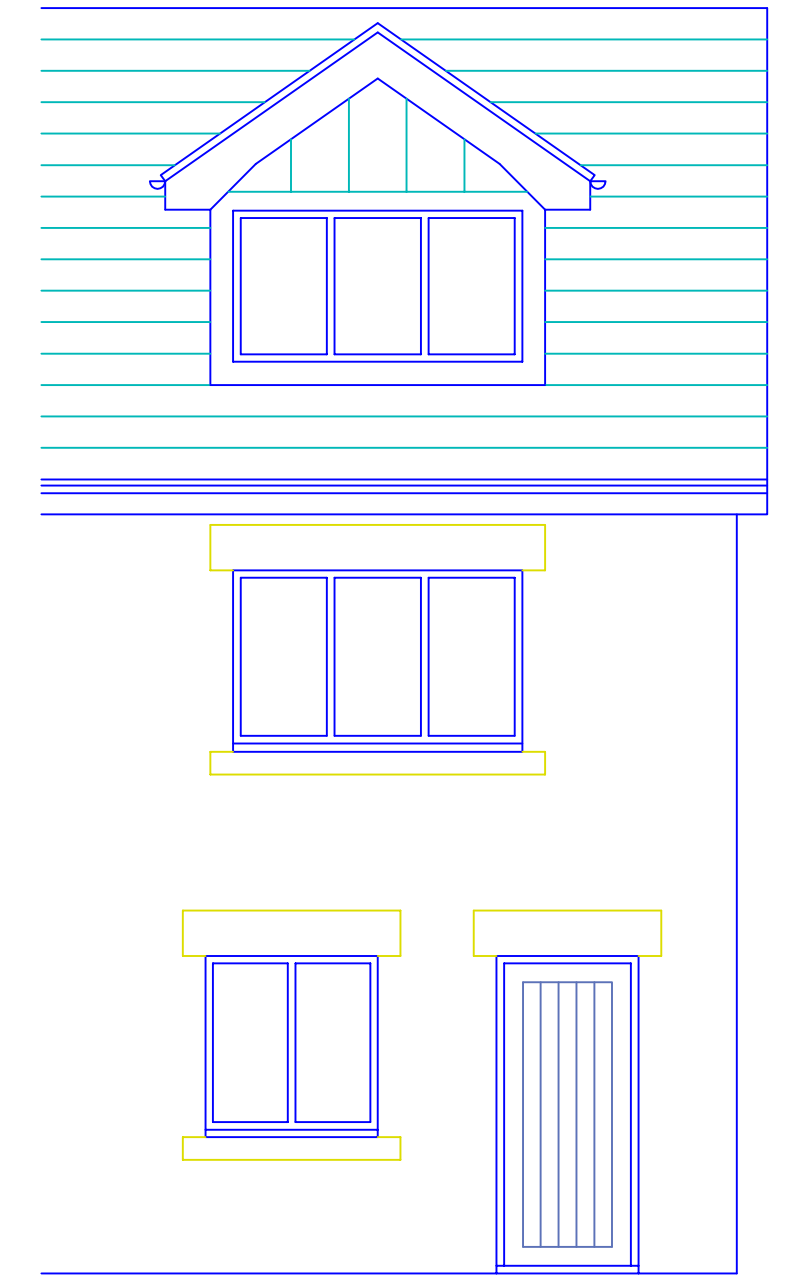
PROPOSED SIDE ELEVATION



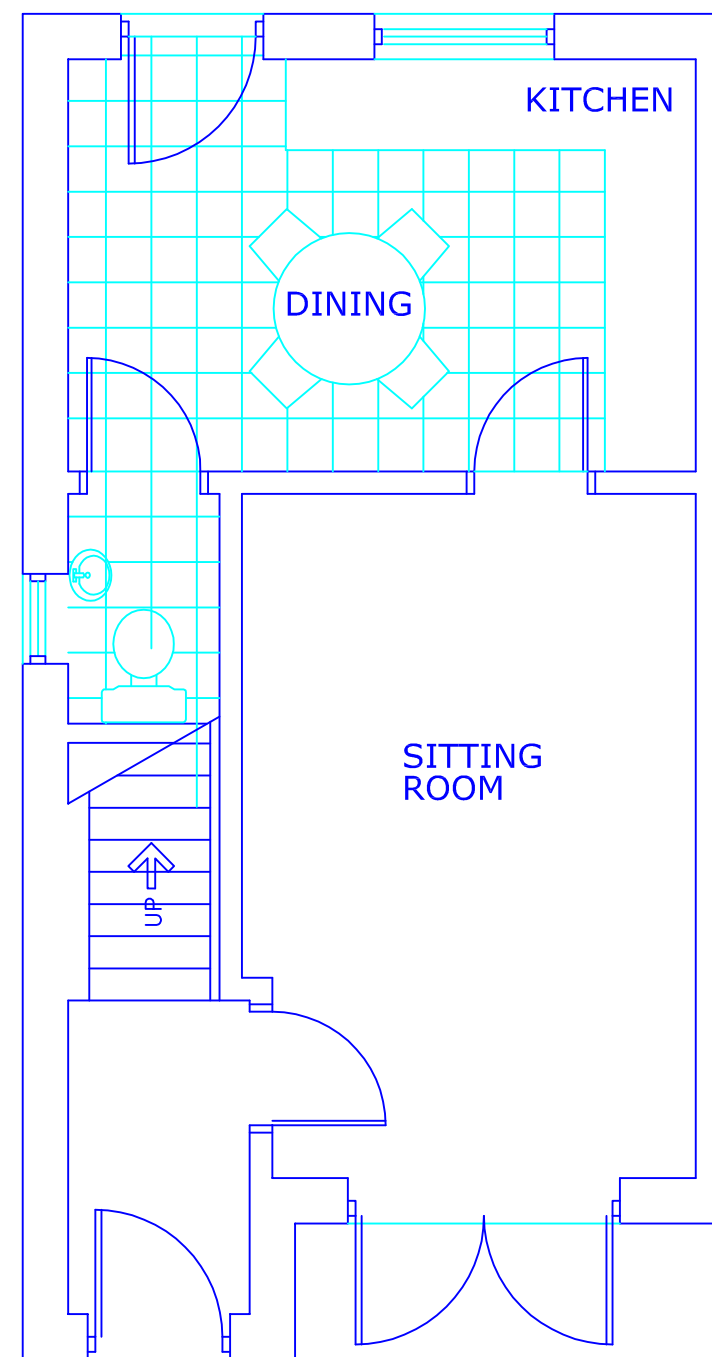
PROPOSED FRONT ELEVATION



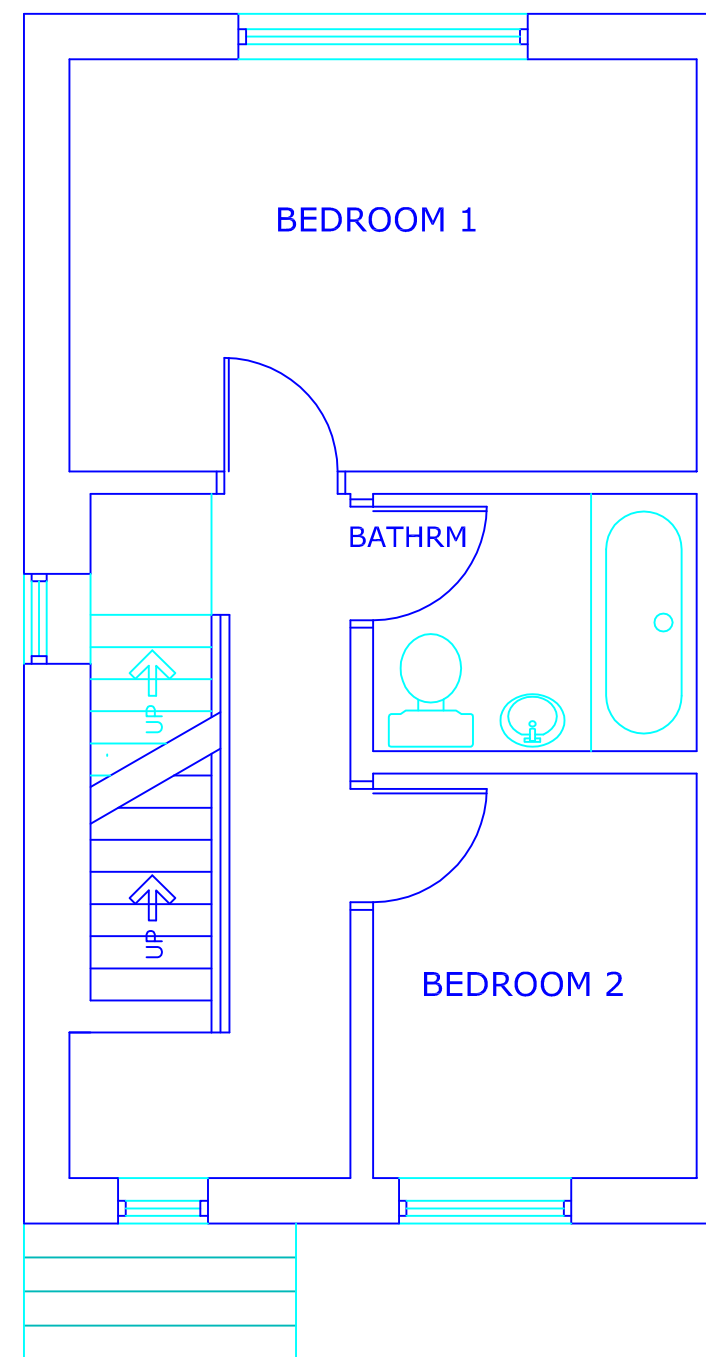
PROPOSED SIDE ELEVATION



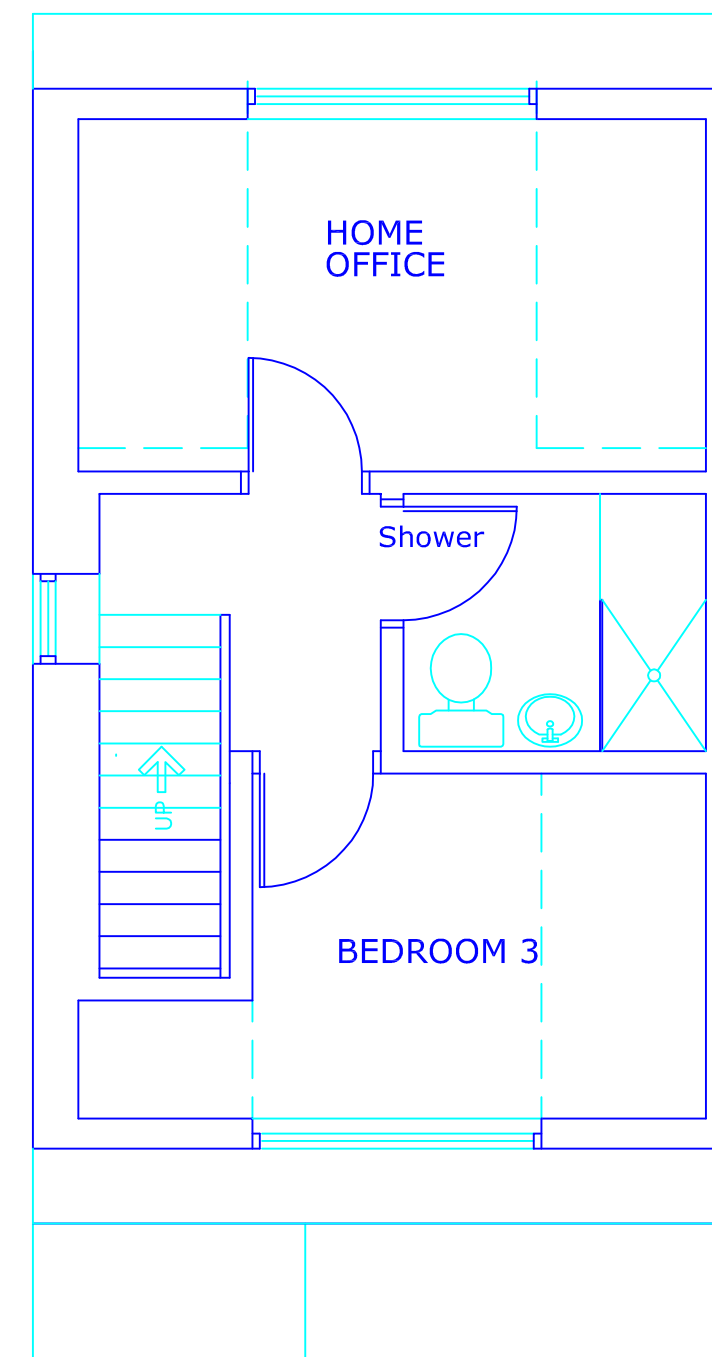
PROPOSED REAR ELEVATION



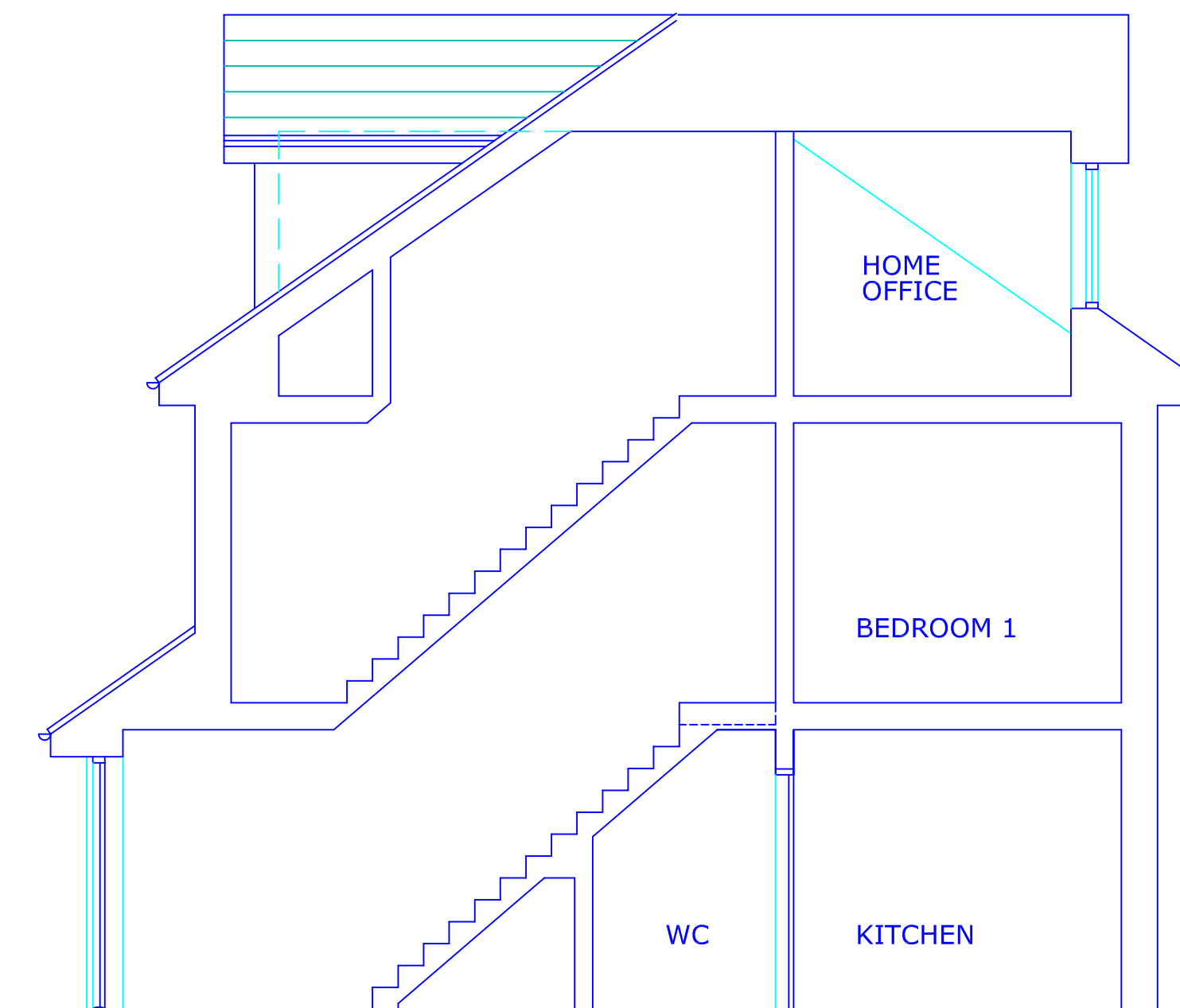
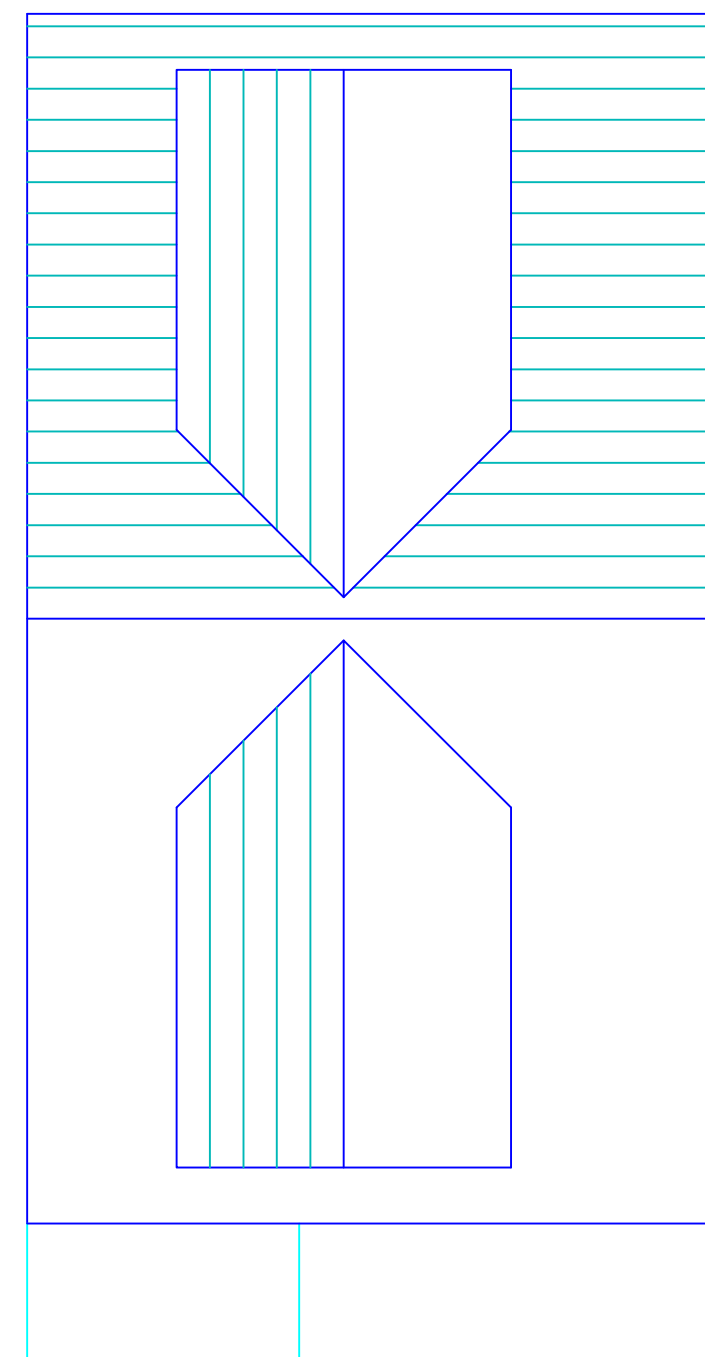
PROPOSED PLAN
GROUND FLOOR



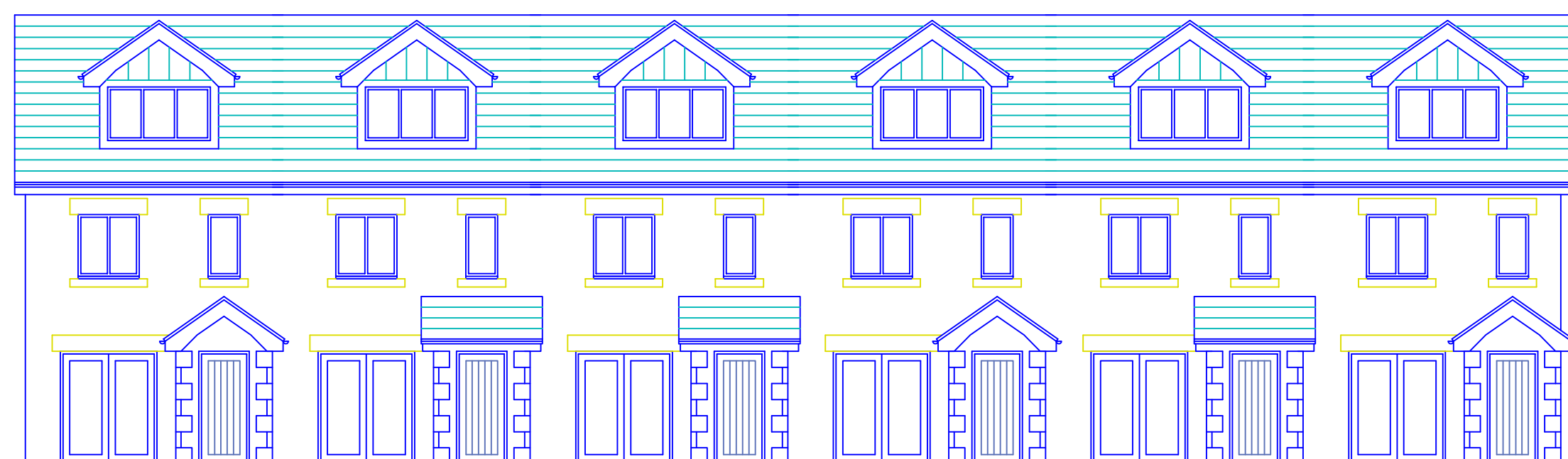
PROPOSED PLAN
FIRST FLOOR



PROPOSED PLAN
SECOND FLOOR



PROPOSED SECTION



PROPOSED
STREET ELEVATION

A Windows added to side elevations 26.09.2016

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0 1 2 3 M

Land at 77 Bury Road Radcliffe M26 2UT	scale 1:50@A1 April 2015
IG1-03A	House Type A
PPY design Ltd	

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Ward: Prestwich - Sedgley

Item 02

Applicant: Unimore Limited

Location: Texaco Petrol Station, Bury Old Road, Prestwich, Manchester, M25 0EY

Proposal: Demolition of existing structures and erection of an retail (A1) unit and synagogue with associated car parking, landscaping and access arrangement

Application Ref: 60045/Full

Target Date: 02/09/2016

Recommendation: Minded to Approve

This application was presented to Planning Control Committee (PCC) as Councillor Caserta has called the application in. The PCC deferred the application in January 2017 to allow the application to be revisited by the applicant.

This application is Minded to Approve subject to the completion of a Section 106 Agreement to secure the implementation of a Resident's Parking Permit Scheme pursuant to Policy HT2/4 - Car Parking and New Development and SPD11 - Parking Standards in Bury

Description

The site currently contains a petrol filling station, associated shop and car wash. The site is located centrally within a local shopping centre with access from Bury Old Road. The petrol pumps are located near to Bury Old Road with the shop and car wash located to the rear.

There are commercial buildings to the north and south, which front onto Bury Old Road. There are residential properties to the north (Albert Road), east and west with allotments beyond.

Planning permission was granted in October 2013 for the demolition/removal of the shop, car wash and petrol pumps and the erection of a single storey building containing an Class A1 retail unit with an ancillary Class A3 cafe unit (combined floor space of 418 square metres). This consent has not been implemented.

The proposed development has been revised to the demolition/removal of the existing shop, car wash and petrol pumps and the erection of a two storey building containing a single retail (Class A1) unit at ground floor and a synagogue/shull at first floor. A new access would be formed onto Bury Old Road, which would connect to the car park, with 15 spaces.

The proposed opening hours for the proposed retail units would be from 08.00 to 23.00 on Monday to Friday and from 08.00 to 16.00 on Sundays. The retail unit would be closed on Saturdays. The opening hours for the proposed synagogue would be from 07.00 to 08.00 and from 19.00 to 22.00 on Monday to Friday, 08.00 to 21.00 on Saturdays and from 08.00 to 09.00 on Sunday. This congregation currently operate at Park View Road and are seeking an alternative venue/site.

Relevant Planning History

33851 - Redevelopment of existing petrol filling station at 19 Bury Old Road, Prestwich. Approved with conditions - 19 March 1998

42819 - Prior approval determination - 14.7m high streetworks pole style telecommunications structure accommodating 3 no./ shrouded antennae and 2 equipment cabins at site outside 19 Bury Old Road, Prestwich. Refused - 13 July 2004.

53747 - Prior notification for 14.8 metre high streetworks pole including 3 antennae and equipment cabinets at Prestwich Service Station, 19 Bury Old Road, Prestwich. Prior approval required and granted - 19 April 2011.

56277 - Demolition of existing structures and erection of A1 retail unit with ancillary A3 use and associated car parking, landscaping and access arrangement at Texaco Petrol Station, Bury Old Road, Prestwich. Withdrawn - 15 July 2013.

This application was withdrawn as there were issues relating to the access and servicing arrangements.

56526 - Demolition of existing structures and erection of an A1 Use Class retail unit, with ancillary A3 use and associated car parking, landscaping and access arrangement (resubmission of 56277) at Texaco petrol station, Bury Old Road, Prestwich. Approved with conditions - 10 October 2013.

Publicity

The neighbouring properties were notified by means of a letter on 8 June and a press notice was published in the Bury Times on 16 June 2016. Site notices were posted on 15 June 2016.

A letter of support has been received from Councillor Quinn, which has raised the following issues:

- Support the application as I know the congregation well and am a governor of nearby Park View School.
- They are a small congregation and have been excellent and considerate neighbours to the school.
- In 20 years I have never seen them use cars to attend the synagogue.
- Wish them every success with their application.

112 letters of support have been received from the occupiers of the following properties:

PRESTWICH - 1, 8, 18, 19 Ravensway, 20 George Street, 19 Castle Hill Road, 47, 76, 76A, 105, 122 Kings Road, 9 Ravens Close, Park Hill (5), 8, 12, 30 Bury Old Road, 12 Dellcot Close, 5, 12, 16 Salisbury Drive, 90 Glebelands Road, 1 Lancaster Drive, 3, 5, 17 Hereford Drive, 27 Hilton Crescent, 66A, 80, 118, 180 Albert Avenue, 17 Woodland Crescent, 38 Bishops Road, 7, 17 Lichfield Drive, 4 Lancaster Drive, 5, 6, 45 Tewkesbury Drive, 2B Mather Avenue, 4 Parksway, 3, 11, 15 Sedgley Park Road, 14 Canterbury Drive, 16 Breeze Mount, 7, 66 Richmond Avenue, 14, 16 East Meade, 14 Silverdale Avenue, 15 Sedgley Avenue, 8 Barnhill Avenue, 8, 15, 38, 68, 82 Park Road, 3 Oakfield, 2 Links Crescent, 7 Harrogate Avenue.

WHITEFIELD - 37 Standmoor Road.

SALFORD - 11 Vernon Road, 1, 11 Park Lane, 19 Old Hall Lane, 55 Broom Lane, 20 The Mount, 8, 22, 27, 38, 77, 88, 97 Cavendish Road, Flat 2, Grosvenor Court, 11 New Hall Avenue, 13 Sommerville Court, Flat 4, Hazel Slack, Limefield Court, 55, 58 Upper Park Road, 18 Eccleston Place, 397 Great Cheetham Street East, 34 Stanley Road, 10 Eskrigge Close, 18 Rowan Way, Flat 7, Ingledene Court, Manchester & District Council of Synagogues, Holden Road, 2A Worthington Drive, 468B Bury New Road, 13 The Vineyard, Flat 2, 4, Bedford Court,

MANCHESTER - 23, 38, 54 Park Road, New Hall Road, 6, 8 Kersal Crag, 2 Deanery Gardens, 57 - 59, 95 Leicester Road, 75, 126 Nevile Road, 8 Waterpark Road, Manchester Beth Din, Bury Old Road, 97 Singleton Road,

CRUMPSALL - 8 Melton Road,

GATESHEAD - 178 Prince Consort Road,

The letters of support have raised the following issues:

- The relocation would provide a fantastic springboard for all kinds of communal activities for the benefit of local residents - youth groups, prayer services, public lectures, charity drives, academic research facilities and hosting of community support services.
- Believe this building will serve a tremendous need.

- A convenience supermarket would be of benefit to the wider public.
- Objections based upon traffic are clearly unfounded as the area would not be any busier than current usage.
- Have been a member of this synagogue for 40 years but can no longer attend as it is too far for me to walk. The proposed site would be within walking distance of my current home.
- Been a member for 15 years and attended for over 30 years. Over this time the synagogue has lost members as it is on the periphery of the community.
- As it is a small building, there are no adequate facilities for young children, which means we lose young married.
- Park View Road is becoming increasingly congested making walking there on the Sabbath and other Holy Days is becoming difficult.
- We have received a lot of positive comments from potential members.
- This ideal location will enable elderly people and kids who can't walk far to attend services and social programs arranged by the synagogue.
- We need a local synagogue within a convenient walking distance as local demographic changes have resulted in a lack of such facilities.
- The Jewish community lives much closer to the proposed site and it will enhance the continuity of the synagogue if permission is granted.
- There would be no congestion at the site on the sabbath.
- As the shop and Synagogue will be open less hours than the existing filling station there would be less stress on the local roads than there is now.
- There is a need for a local kosher store, which would be of use for those without vehicles.
- The proposal would provide a new centre for our youth in an age where facilities for them are in decline.
- The local area has a dramatically expanding Jewish Orthodox population, which needs to be catered for. The proposed development would help to service the needs of the Jewish community.
- The Jewish community is growing quickly and there is a desperate need for more synagogues and kosher supermarkets. We are grateful for your ongoing support to ensure that the community can grow.
- Moreover, to relocate the synagogue from its current location will undoubtedly encourage more to join.
- The proposed development would increase footfall in the area.
- The proposed development would improve the existing derelict site.

6 letters have been received from the occupiers of 137, 180 Albert Avenue, 6 St Mary's Road, 6 Park Hill Bury Old Road, 1 Ravensway, which have raised the following issues:

- Concerned about parking, which is a real difficulty at the moment.
- Many people from that block who work/shop there park on Albert Avenue, which is inconvenient for residents.
- I would be ok with the plans if the Council made Albert Avenue a residents only parking space.
- Overall massing/scale of development is out of character with the adjacent buildings.
- Privacy distances between proposed building and existing dwellings seem inadequate.
- Parking facilities are inadequate and would add to existing congestion.
- Proposed building would be 10.8 metres as my garden is 1.8 metres lower than the site.
- Concerns relating to operating hours, light pollution and noise pollution.
- The synagogue does not need to move. It is fine where it is situated now - waste of money.

The objectors and supporters were notified of revised information by means of a letter on

A letter has been received from Cllr Quinn, which has raised the following issues:

- In my view, the application should be approved by the Planning Committee.
- All the conditions have been met and I'd like to thank officers for their assistance in

dealing with the application.

- The congregation live near to the proposed Shull and this will make their lives easier, especially on the Shabbatt.
- This is a small congregation and parking will not pose a problem now we have a traffic management plan.

1 letters of support has been received from the occupiers of Flat 7, Ingledene Court, which has raised the following issues:

- Having read the new documents, I wish to reaffirm my support for the move.
- It will reduce my walking time on the Sabbath and Holy Days from 60 minutes to 15 minutes and means I can attend more often.
- Based on 28 years membership of the synagogue I can confirm that the statements made in the application regarding useage of the premises are accurate and that the issue of parking has been adequately dealt with.

1 letter have been received from the occupiers of 127 Abert Avenue, 28 Bury Old Road and 1 Ravensway, which have raised the following issues:

- There are enough shops and offices on Bury Old Road, which cause a lot of congestion and it is difficult for residents to park on our own street.
- If you make Albert Avenue a residence only parking area, the above plans wold not affect me.
- Strongly object as traffic is very bad and it is an accident blackspot.
- The development is lit up like Blackpool's golden mile and it is difficult to sleep in the front bedroom.
- There are a large number of supporters who live far away and some are claiming to be local residents. This makes a mockery of the whole consultation process.
- The new parking will do little to aid the free movement of traffic on Bury Old Road.
- Double yellow lines are ignored and we cannot see anyone parking and walking from the Maccabi Centre.
- The retail unit may take trade from other businesses.
- We remain concerned about security having a synagogue on both sides of our home.
- Concerned as Mr Rabinowitz has stated that he lives at our address, which is not true.

The objectors and supporters have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objections in principle and further comments to be reported in the Supplementary Report.

Drainage Section - Comments awaited.

Environmental Health - Contaminated Land - No objections, subject to the inclusion of conditions relating to contaminated land.

Environmental Health - Commercial Section - Comments awaited.

Environmental Health - Pollution Control - Comments awaited.

Waste Management - Comments awaited.

Environment Agency - No objections, subject to the inclusion of conditions relating to surface water drainage and contaminated land.

Designforsecurity - No objections.

United Utilities - No objections, subject to the inclusion of conditions relating to foul and surface water drainage.

GM Fire Service - Comments awaited.

GM Ecology Unit - No objections, subject to the inclusion of informatives relating to bats and nesting birds.

National Grid - Comments awaited.

Unitary Development Plan and Policies

EN1/1 Visual Amenity

EN1/2 Townscape and Built Design

EN1/3	Landscaping Provision
EN1/4	Street Furniture
EN1/5	Crime Prevention
EN1/7	Throughroutes and Gateways
EN1/8	Shop Fronts
EN6	Conservation of the Natural Environment
EN6/3	Features of Ecological Value
EN7	Pollution Control
EN7/2	Noise Pollution
EN7/5	Waste Water Management
S1/4	Local Shopping Centres
S2/1	All New Retail Proposals: Assessment Criteria
S4/4	Car Showrooms, Car Sales Areas and Petrol Filling Stns
HT2/4	Car Parking and New Development
HT4	New Development
HT5/1	Access For Those with Special Needs
CF1/1	Location of New Community Facilities
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle (retail) - Policy S1/4 states that the Council will seek to maintain and enhance local shopping centres and will encourage the provision of a range of shopping facilities required to serve purely local needs.

Policy S2/1, which states that the Council will support new retail development which accord with the following:

- are within or immediately adjoining the main shopping area of existing centres;
- sustain or enhance the vitality or viability of a centre;
- are accessible by and would encourage greater use by public transport;
- are in conformity with other policies and proposals of the Plan

In addition, where a proposal would lead to more than 40% of the frontage being in non-retail use, the Council will take into account the following factors:

- the location and prominence of the proposal within the frontage;
- the number, distribution and proximity of other premises in non-retail use;
- the particular nature and character of the use proposed, including the level of activity associated with it.

The proposed development was originally 2 units and has been amended to provide a single retail unit at ground floor level.

The application site lies within the local shopping centre and would have a floorspace of 467 square metres. The proposed retail unit would act as an anchor for the centre and provide linked trips, which would generate trade for existing businesses. As such, the proposed development would have a positive impact upon the vitality and viability of the centre. Therefore, the proposed development would be in accordance with Policies S1/4 and S2/1 of the Bury Unitary Development Plan.

Principle (Community use) - Policy CF1/1 states that proposals for new and improved

community facilities will be considered with regard to the impact upon residential amenity and the local environment; traffic generation and car parking; the scale and size of the development; accessibility by public and private transport; the needs and requirements of the disabled.

The proposed development would provide a synagogue at first floor level. The site would be accessible by public and private transport and would be located in the local area, it would be intended to serve. The issues of residential amenity, traffic generation, the scale and size of the development and the needs and requirements of the disabled will be discussed below. Therefore, the proposed development would be acceptable in principle and would be in accordance with Policies CF1/1 of the Bury Unitary Development Plan.

Design and layout - The proposed building accommodating two uses, would be two storeys in height and would be located towards the rear of the site. The proposed building would be 9 metres in height and would be some 1.4 metres higher than the existing buildings. However, the existing buildings would be located towards the front of the site and given the distance between them, the difference in height would not be perceptible. The levels with the neighbouring properties have been checked and are as shown.

The proposed development would incorporate a modern design with floor to ceiling glazing for the retail units on the front elevation as a focal point. The proposed building would be constructed from timber cladding, grey brickwork and render, which would be acceptable and would add visual interest to the elevations. Therefore, the proposed development would not be a prominent feature in the streetscene and would be in accordance with Policies EN1/2 and S2/1 of the Bury Unitary Development Plan.

Impact upon surrounding area - The proposed site plan indicates that the trees on the perimeter of the site would be retained and a condition relating to tree protection measures would be added to secure this. The proposed bin store would be located in the western corner of the site and would be accessed from the loading bay in the car park. A timber fence would be erected to the boundaries of the site, which would not be prominent in the locality. Therefore, the proposed development would be in accordance with Policies EN1/2 and S2/1 of the Bury Unitary Development Plan.

Impact upon residential amenity - SPD6 provides guidance on aspect standards and as such, is a useful guide in this instance. The aspect standards states that there should be a minimum of 20 metres between directly facing habitable windows and 13 metres between a habitable room window and a two storey blank wall. If there is an additional storey in height, a further 3 metres should be added to the aspect standard.

The proposed building would be 9 metres in height, which would be the equivalent of a three storey building. There would be between 16.75 metres and 17.65 metres between the residential properties on Albert Avenue and the gable elevation of the proposed building. Whilst there are 6 windows within the gable elevation of the proposed building, these would be obscure glazed and non-openable. Therefore, the proposed development would be in excess of the 16 metre aspect standard and would not have an adverse impact upon the amenity of the neighbouring properties. Although the massing of the building would be somewhat different to that of a house.

Noise - There is a petrol station and hand car wash on site currently and there are no restrictions in terms of hours of opening. The proposed development would involve the provision of a retail unit, which would be open from 08.00 to 23.00 on Mondays to Fridays and from 08.00 to 16.00 on Sundays. The applicant is not applying to be open on Saturdays for religious reasons. The main sources of noise would be from cars and deliveries to the site and the proposed levels of noise would be significantly lower than the existing uses at the site. It is proposed to restrict the hours of delivery to 08.00 to 20.00, which would reduce noise levels further. Therefore, it is considered that subject to conditional control, the proposed development would not have a significantly adverse impact upon the neighbouring properties. Therefore, the proposed development would be in accordance with

Policy EN7/2 of the Bury Unitary Development Plan.

Ecology/bats - A bat survey was submitted as part of the application and concludes that the building is completely free of bat roosting potential. It continues that even if demolition is delayed it is unlikely that this building will develop significant roosting potential within the next few years. GM Ecology Unit is satisfied with the conclusions of the report and has no objections to the proposed development, subject to the inclusion of informatives in relation to bats and nesting birds. Therefore, the proposed development would not cause harm to a protected species and would be in accordance with Policy EN6/4 of the Bury Unitary Development Plan and the NPPF.

Highways issues - The northern access to the site would be modified to act as the sole access into the application site. The southern access would be retained as the access to the rear of the adjacent terrace of shops. The Traffic Section has assessed the proposed access previously and has no objections to the access arrangements for the proposed development, subject to the completion of a legal agreement relating to the servicing arrangements. Further comments will be reported in the Supplementary Report. Therefore, this aspect of the proposed development would not be detrimental to highway safety and would be in accordance with Policy S2/1 of the Bury Unitary Development Plan.

Parking - SPD11 states that the maximum parking standards for a retail unit is 1 space per 30 square metres and for a place of worship is 1 space per 5 square metres of public floor area. This equates to 15 spaces for the retail units and 65 spaces for the synagogue (considering the public areas involved only), making a maximum of 80 spaces.

The proposed development would provide 15 parking spaces including 2 disabled parking bays, which would be below the maximum level for the combined development.

The Transport Statement confirms that the synagogue would serve members of the Orthodox community, which does not allow for the use of any kind of mechanically propelled transport vehicles on a Sabbath or a Holy Day. As such, the members of the synagogue would have to walk to the synagogue on the Sabbath or a Holy Day. However,

However, whilst the strict faith restriction and requirements on the Sabbath are noted, the ability to adequately enforce such a position through the planning system is difficult, if not impossible. Such a point may however be academic for the purposes of this planning application, as it is noted that the synagogue would offer prayer services on all other days.

Following the deferral, the Local Planning Authority and the applicant met to discuss. As a result, an Addendum was received, which has sought to clarify a number of queries in relation to the proposal and the operations of the synagogue. The addendum has provided a map showing the catchment area for the proposed synagogue. This indicates that 84% of the families which regularly attend are within a 10 minute walking distance of the proposed site. As such, the proposed development would facilitate walking to the site by the congregation.

The proposed synagogue would be 15% larger than the current synagogue, which would allow for a modest increase in the congregation. Clarification has been provided over the use of the synagogue, which would be for prayer services, with no weddings or large functions. The adjacent room would be used for childrens services, light refreshments after a service and for an occasional lecture in the lead up to a religious festival and would be ancillary to the main use as a synagogue.

There would be some overlap between the retail and religious uses on weekdays. The addendum confirms that most people would walk to the proposed synagogue but if parking is required, an agreement with Maccabi has been completed to allow overflow parking. In addition, there is a public car park at the former Brooklands library, which could be used if required.

Whilst the applicant has made a case for the obvious shortfall when compared to the maximum parking provision and has identified alternative parking arrangements, the Traffic Section has a concern that the congregation could still park on the residential streets in the vicinity, where there is already a reliance upon on-street parking and may leave their vehicles before the Sabbath commences and collect their vehicles once it is over, creating parking concerns (considering alternative users of the building).

A solution which has been put forward to resolve this would be to bind the applicant to funding a residents parking scheme (if the parking issue arises) which would be secured by way of a legal agreement, to which the applicant has agreed.

The general terms of the agreement would seek a financial contribution from the developer to be held by the Council to implement a Resident's Parking Scheme if considered necessary. This would be for a total amount of £15,000 which would include pre and post development monitoring surveys and implementation of a scheme. Details of the scheme are considered in the Planning Obligations section below.

Given that there would be more certainty that mitigation measures could be put in place if parking in the area became a problem as a result of the development, the Traffic Section would be in a position to support the application subject to conditions and the signing and completion of the legal agreement.

Therefore, the proposed development would provide an adequate level of car parking provision and would not have an adverse impact upon the safe and efficient operation of the highway network. Therefore, the proposed development would be in accordance with Policies EN1/2, EN1/7, HT2/4, HT4 and CF1/1 of the Bury Unitary Development Plan and SPD11.

Access - Level access would be provided to the building and two disabled parking bays would be located in close proximity to the entrance. A lift would be provided within the building and a disabled toilet would be provided at first floor level. Therefore, the proposed development would be accessible for all and would be in accordance with Policies HT5/1 and CF1/1 of the Bury Unitary Development Plan.

Planning obligations - A Section 106 Agreement would secure a contribution of £15,000 to implement a residents parking scheme (if considered necessary) and to restrict the servicing within the site to a transit van sized vehicle as this is the largest vehicle that could operate in the turning head.

The terms of the obligation can be summarised as follows:

- £15,000 in total - £5,000 monitoring/£10,000 implementation.
- Monitoring to include pre commencement of development and post occupation of development parking surveys at suitable times of the week, to be carried out by TfGM or the Developer
- To include a repayment clause for any unspent contributions to be returned to the Developer within a reasonable timeframe (5 years)

There is an element of risk involved by incorporating a timescale. The Council may find itself in a position where it has refunded the contribution to the developer but should the denomination of the religious faith change which could happen without the need for planning permission, this may have a greater impact on parking. However, it would be unreasonable and unwarranted to expect the Developer to be bound for an indefinite period 'just in case' and on this basis fail the legal test. A restricted period does not.

As the development involves a purpose built Synagogue, specifically designed and laid out for the Jewish faith, it is less likely to be used for another purpose or by another denomination in the Borough. There are no other examples in the Borough where this has been the case. The Traffic Section is prepared to accept this risk.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Minded to Approve

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered DA14079.001.2, DA14079.002, DA14079.003.4, DA14079.004.1 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details/Samples of the (materials/bricks) to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
4. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. Following the provisions of Condition 4 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

6. Piling or any other foundation designs using penetrative methods shall not be permitted other than with the prior written consent of the Local Planning Authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to groundwater. The development shall be carried out in accordance with the approved details.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

7. No infiltration of surface water drainage into the ground where adverse concentrations of land contamination are known or thought to be present is permitted other than with the prior written consent of the Local Planning Authority, which may be given for those parts of the site where it has been demonstrated that there is no resultant unacceptable risk to controlled waters. The development shall be carried out in accordance with the approval details.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

8. Foul and surface water shall be drained on separate systems.

Reason. To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution pursuant to Policy EN7/5 - Waste Water Management of the Bury Unitary Development Plan and Section 10 of the National Planning Policy Framework.

9. Prior to the commencement of any development, a surface water drainage scheme, based on the hierarchy of drainage options in the National Planning Practice Guidance with evidence of an assessment of the site conditions shall be submitted to and approved in writing by the Local Planning Authority. The surface water drainage scheme must be in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015) or any subsequent replacement national standards and no surface water shall discharge to the public sewerage system either directly or indirectly.

The development shall be completed in accordance with the approved details.

Reason. To promote sustainable development, secure proper drainage and to manage the risk of flooding and pollution pursuant to Policy EN7/5 - Waste Water Management of the Bury Unitary Development Plan and Section 10 of the National Planning Policy Framework.

10. The retail use hereby permitted shall not be open to customers outside the following times:

08.00 to 23.00 Monday to Friday

08.00 to 16.00 Sunday

Reason. To safeguard the amenities of the occupiers of nearby residential accommodation pursuant to Policies S2/1 - All New Retail Proposals: Assessment Criteria of the Bury Unitary Development Plan.

11. The place of worship hereby permitted shall only be occupied and available for use between the following times:

06.30 - 09.00 and 18.30 - 22.30 - Monday to Friday

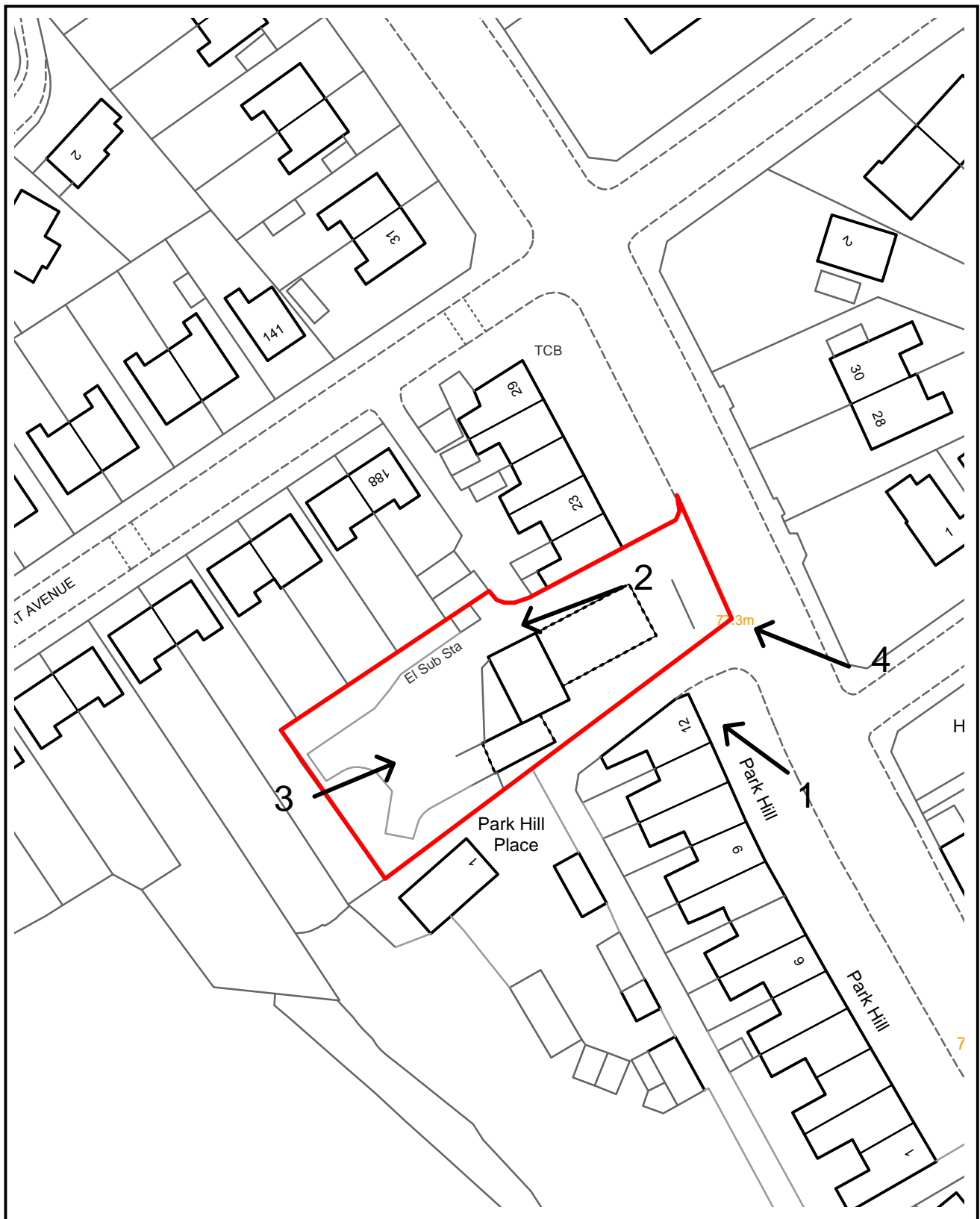
Reason. To ensure there would be no conflict of uses and to secure maximum parking provision is available pursuant to the following Policies of the Bury Unitary Development Plan and SPD11 - Parking Standards in Bury:

Policy H2/4 - Car Parking and New Development
Policy CF1/1 - Location of New Community Facilities

12. No deliveries shall be made to the site outside the hours of 08.00 to 20.00 on a daily basis.
Reason. In the interests of residential amenity pursuant to Policy S2/1 - All New Retail Proposals: Assessment Criteria.
13. All trees to be retained on site shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are implemented and all measures required shall remain in situ until the development has been completed.
Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**

Viewpoints



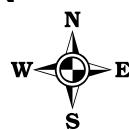
PLANNING APPLICATION LOCATION PLAN

APP. NO 60045

ADDRESS: Former Texaco petrol station,
Bury Old Road, Prestwich

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

60045

Photo 1



Photo 2

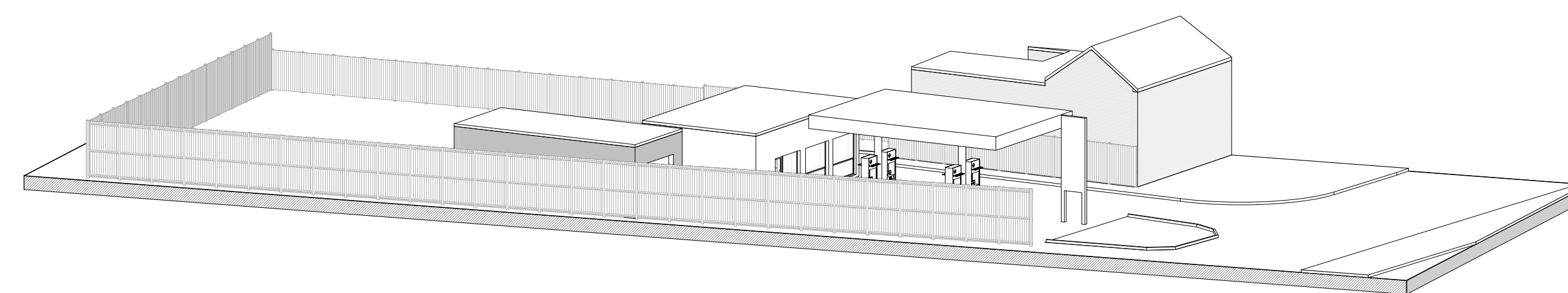
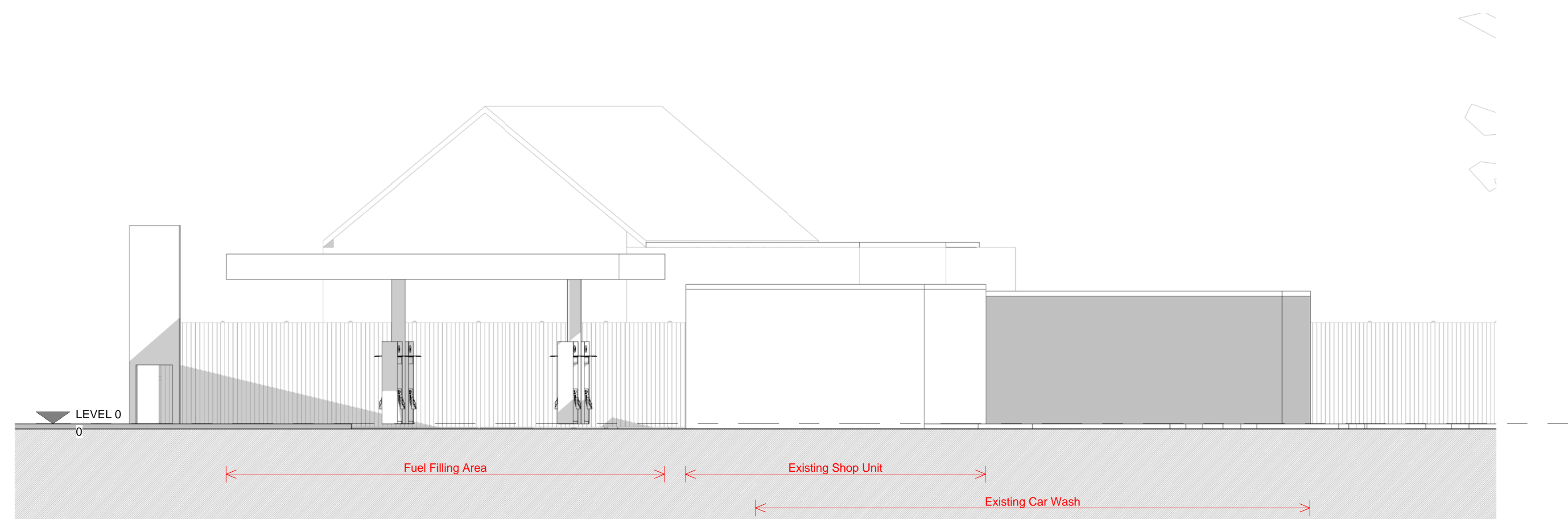
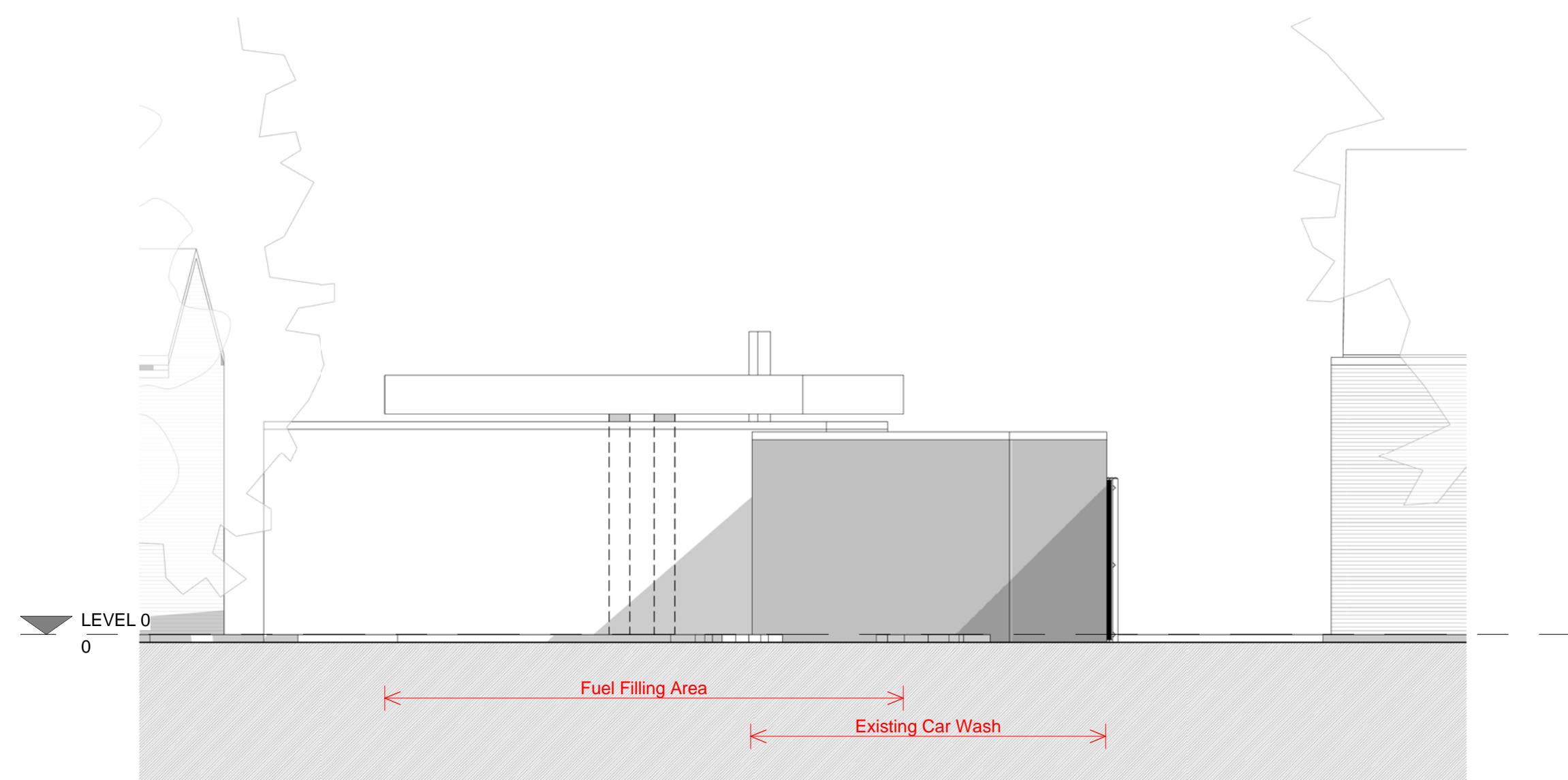
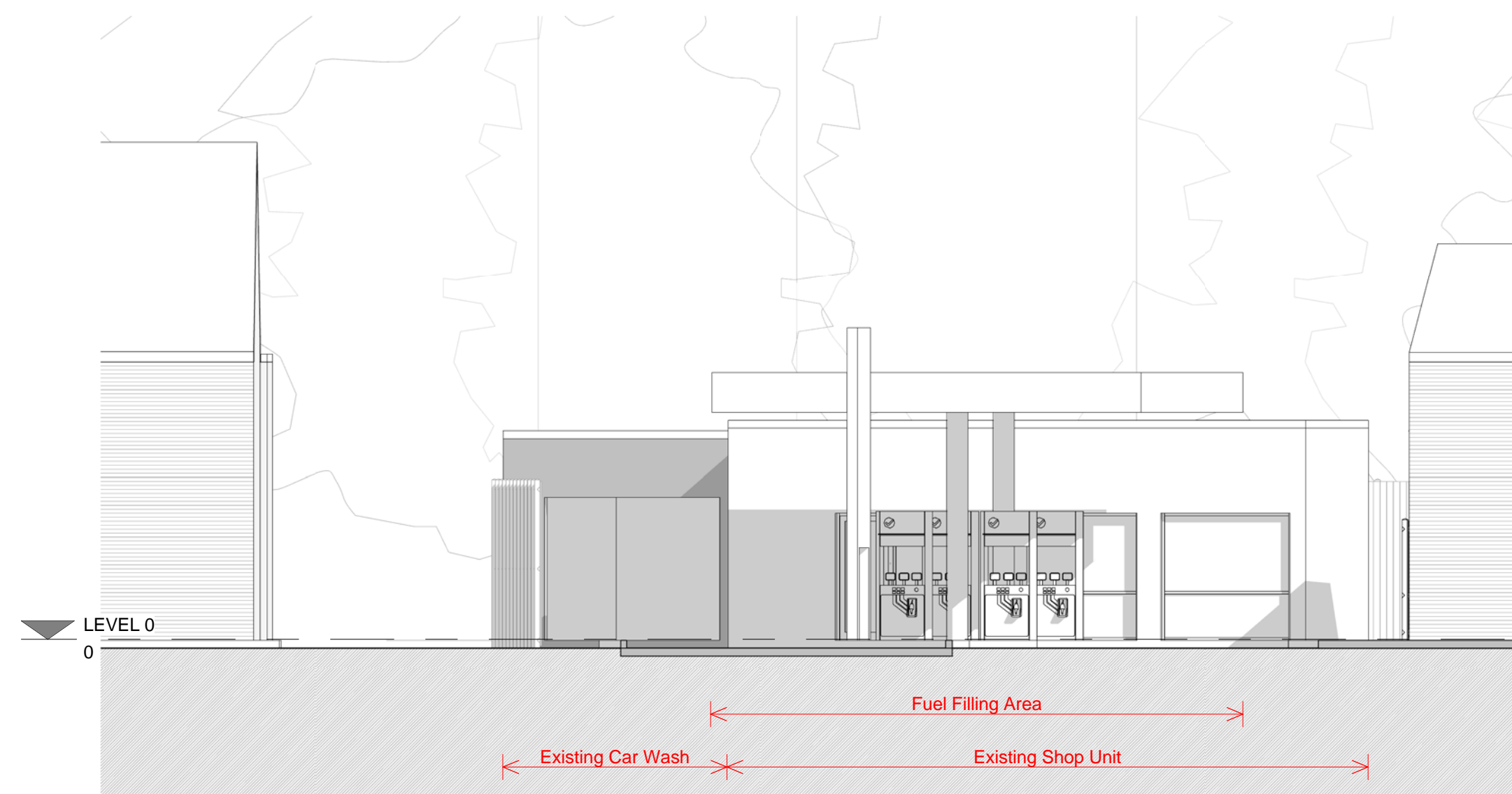


Photo 3

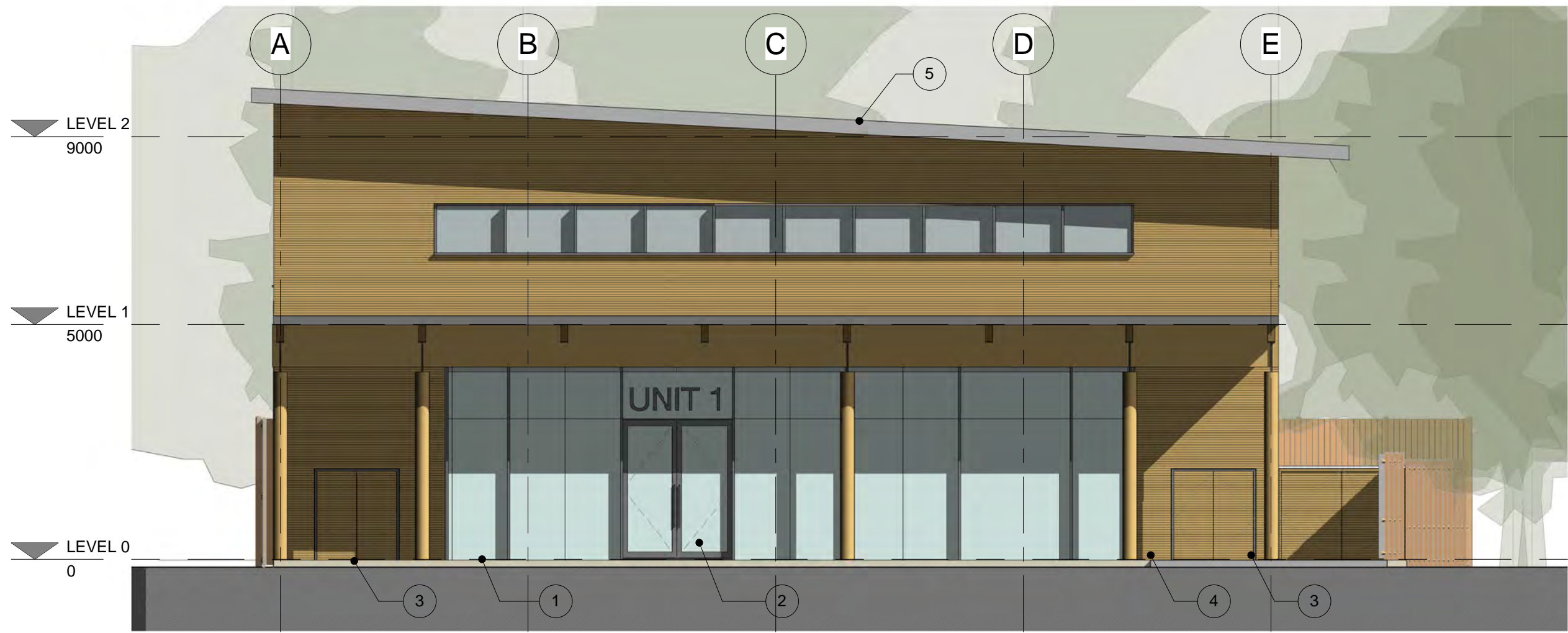


Photo 4

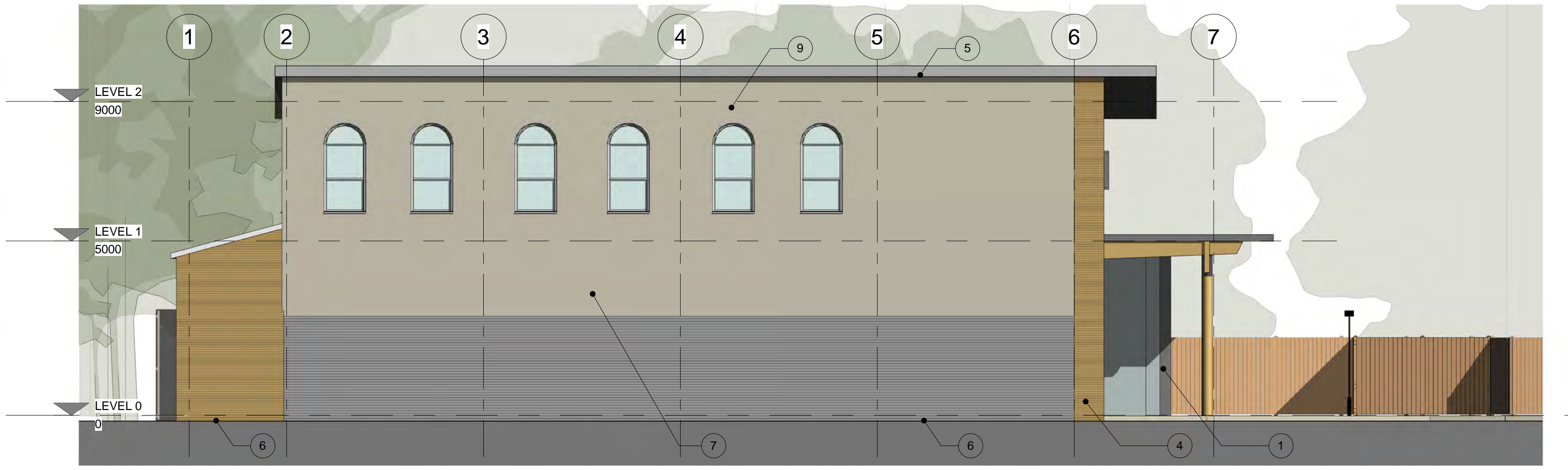




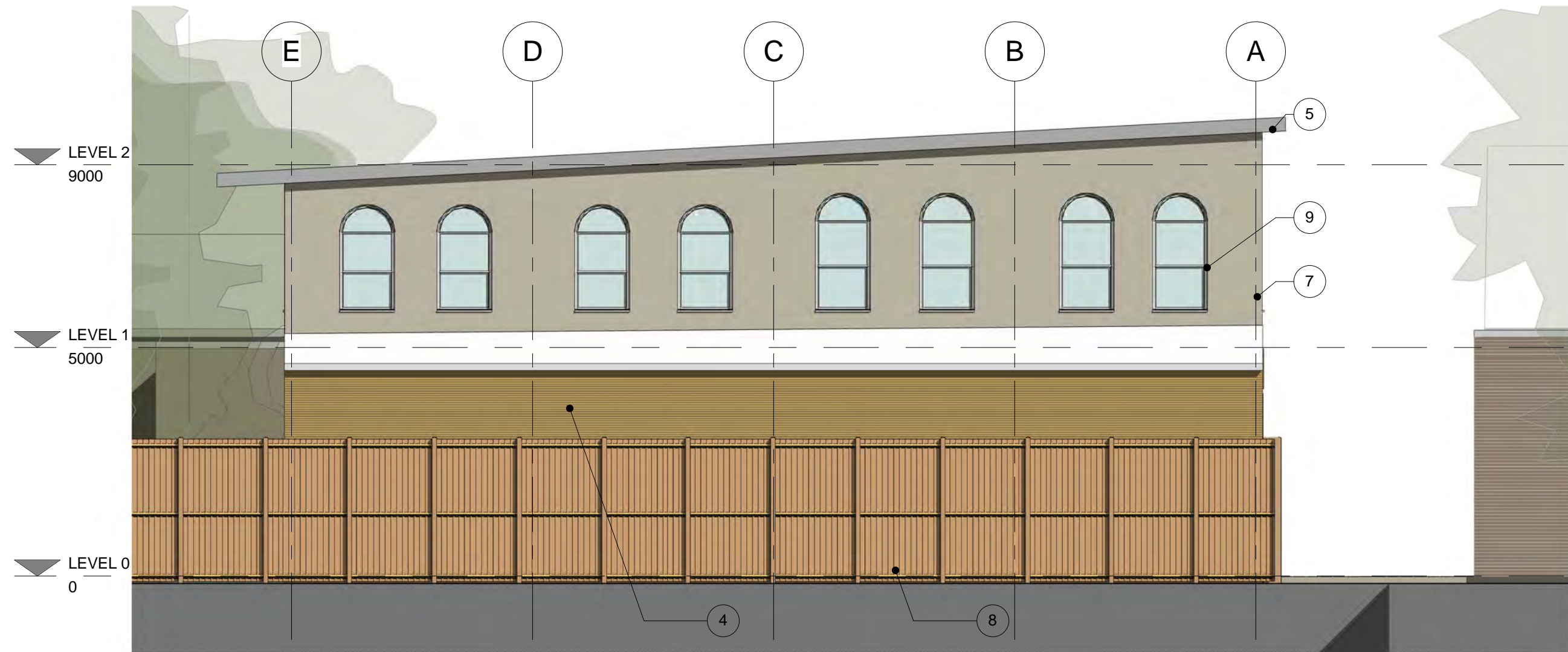
<h1>DEBTAL ARCHITECTURE</h1> <p>72 Bury New Road, Manchester, M20 0JL Tel: 0161 773 1030 www.debtalarchitecture.co.uk</p>			
CLIENT	Unimore Ltd		
PROJECT	Retail Units and Synagogue above		
TITLE	Existing GA Plans and Elevations		
PROJECT NO.	DISPOSAL NO.	REV.	
<h2>DA14079. 002.</h2>			
DATE	PLANNING		
ISSUED	DATE	OFFICE	
	02 FEB 2015		
<p><small>IT IS EMPHASIZED, AS HAS BEEN STATED, THAT THIS DRAWING IS NOT A PROFESSIONAL ARCHITECTURAL DRAWING. IT IS A PRELIMINARY DESIGN STUDY. IT IS NOT TO BE USED FOR CONSTRUCTION OR FOR ANY OTHER PURPOSE. IT IS THE RESPONSIBILITY OF THE CLIENT TO OBTAIN ALL NECESSARY PERMISSIONS AND APPROVALS. THE ARCHITECT ASSUMES NO LIABILITY FOR ANY CONSEQUENCES ARISING FROM THE USE OF THIS DRAWING. THE ARCHITECT'S SERVICES ARE LIMITED TO THE DESIGN OF THE BUILDING. THE ARCHITECT DOES NOT PROVIDE ANY SERVICES IN CONNECTION WITH THE OBTAINING OF ANY PERMITS OR APPROVALS. THE ARCHITECT'S DESIGN IS BASED ON THE INFORMATION PROVIDED BY THE CLIENT. THE ARCHITECT DOES NOT GUARANTEE THE ACCURACY OF THE INFORMATION PROVIDED BY THE CLIENT. THE ARCHITECT'S DESIGN IS SUBJECT TO CHANGE WITHOUT NOTICE. THE ARCHITECT'S DESIGN IS NOT TO BE USED FOR ANY OTHER PURPOSE. THE ARCHITECT'S DESIGN IS NOT TO BE REPRODUCED OR COPIED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM. THE ARCHITECT'S DESIGN IS NOT TO BE USED FOR ANY OTHER PURPOSE. THE ARCHITECT'S DESIGN IS NOT TO BE REPRODUCED OR COPIED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM.</small></p>			



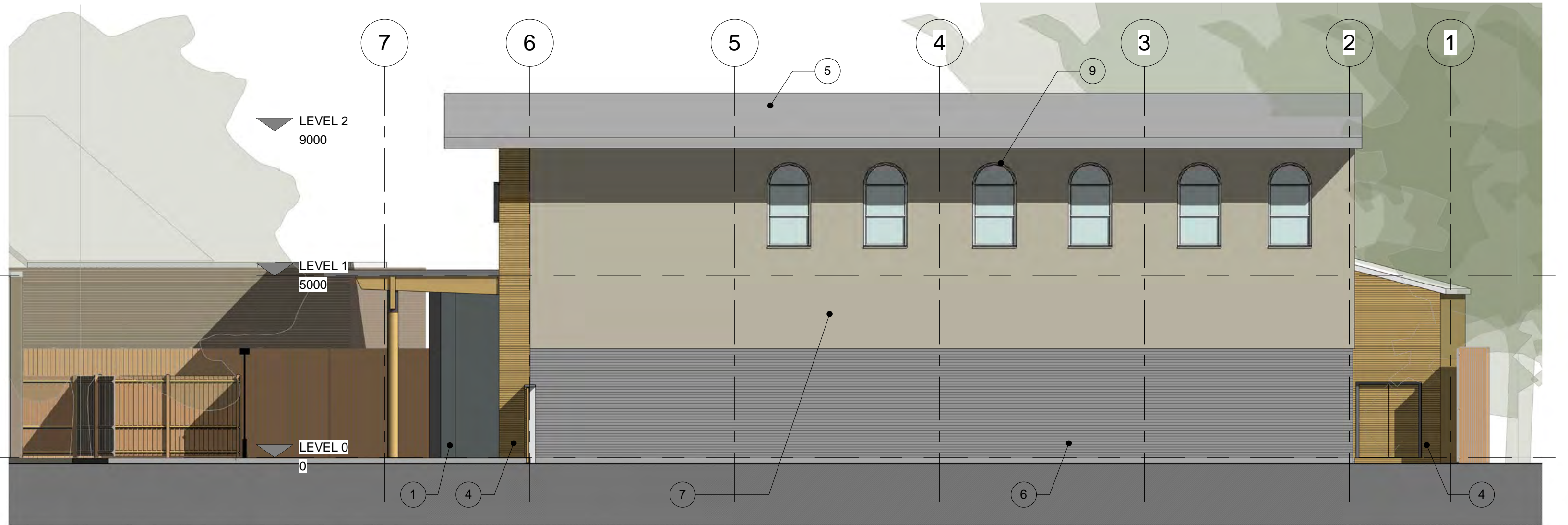
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1 : 100



2 Left Side - Proposed
1 : 100



4 Rear - Proposed
1 : 100



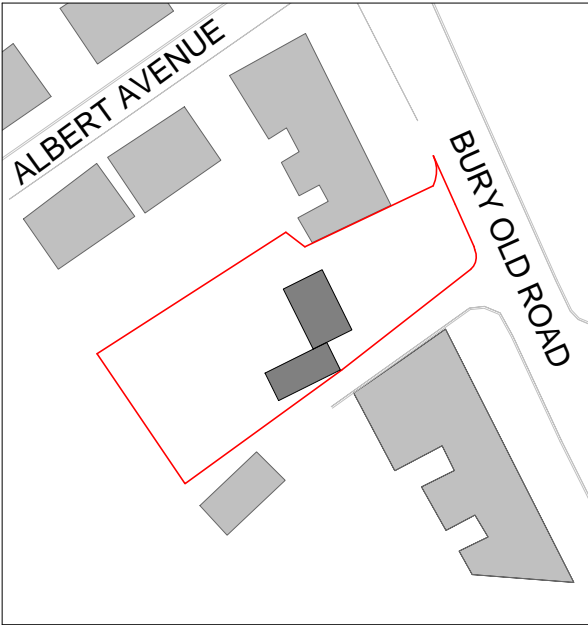
3 Right Side - Proposed
1 : 100



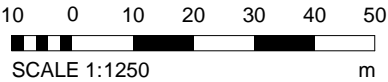
5 3D Axo - Proposed

- Material Schedule
- 1 Proposed Aluminium Curtain Walling with clear and obscured glazing
 - 2 Proposed Shopfront Entrance
 - 3 Proposed Synagogue Entrance
 - 4 Proposed Timber Cladding
 - 5 Proposed Roof
 - 6 Proposed Grey Brickwork
 - 7 Proposed Render
 - 8 Proposed Timber Fencing
 - 9 Proposed Obscured and Non-openable Windows

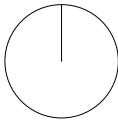
1 Planning Amendments	28/02/17	SP
DEBTAL ARCHITECTURE		
72 Bury New Road, Manchester, M5 0JL Tel: 0161 773 1620 office@debtalarchitecture.co.uk www.debtalarchitecture.co.uk		
CLIENT Unimore Ltd		
PROJECT Retail Units and Synagogue above		
TITLE Proposed GA Elevations		
PROJECT NO. DA14079. 004.	DRAWING NO. 1	REV.
DISCIPLINE PLANNING		
SCALE 1:1 1 : 100	DATE 02 FEB 2015	DESIGNED BY SP
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2 Site - Location Plan
1 : 1250



N



2	Site boundary amended. Exterior lighting spec indicated.	19/05/16	SP
1	Planning Amendments	05/05/16	SP

DEBTAL ARCHITECTURE

72 Bury New Road, Manchester. M25 0JU.
Tel: 0161 773 1630
office@debtalarchitecture.co.uk www.debtalarchitecture.co.uk

CLIENT
Unimore Ltd

PROJECT
Retail Units and Synagogue above

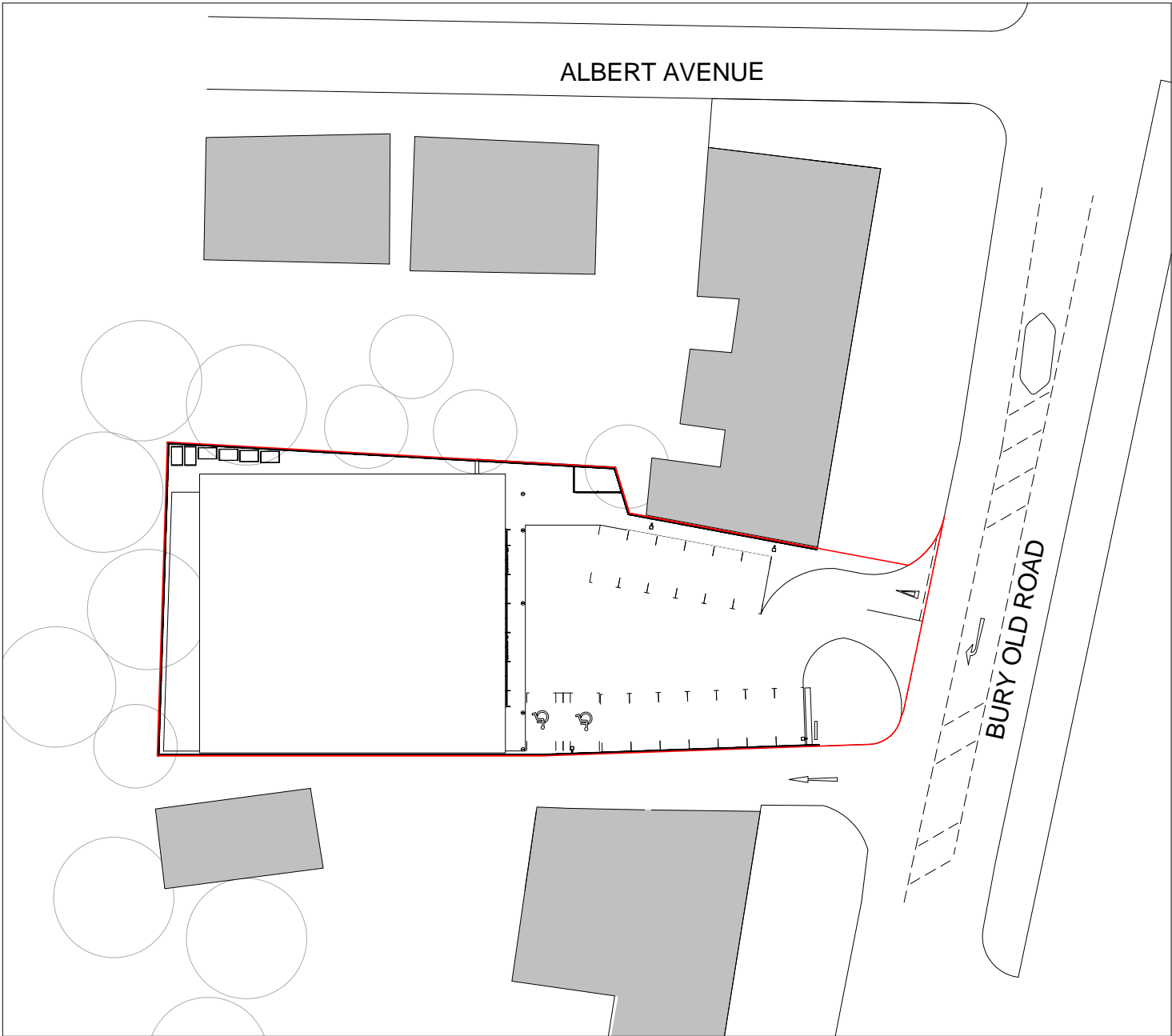
TITLE
Site - Location and Block Plan

PROJECT NO.	DRAWING NO.	REV.
DA14079.	001.	2

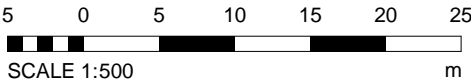
STATUS
PLANNING

SCALE @ A3	DATE	DRAWN BY
As indicated	19 FEB 2015	SP

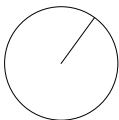
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ARE IN MILLIMETRES (UNLESS OTHERWISE STATED).



1 Site - Block Plan
1 : 500



N



Ward: Bury East

Item 03

Applicant: Mr M Azeem

Location: Land at Shaw Street/Gladstone Street, Bury, BL9 7QD

Proposal: Erection of 3 no. dwellings

Application Ref: 61163/Full

Target Date: 20/04/2017

Recommendation: Approve with Conditions

Description

The proposal relates to a gap site at the end of a row of terraced houses fronting Shaw Street at the junction with Gladstone Street. The site measures 327sqm and is flat. Historic maps indicate there was a social club on the site in the past. The area is residential in character with two storey terraced houses on all sides.

The proposal involves the erection of a row of three 2-bed, two terraced houses fronting Shaw Street. Each house would be two storey and follow the predominant style in the area with conventional brick elevations and a pitched tiled roof. The footprint of each house would be D9m by W4m with eaves at 6.2m and a max ridge height at 9m.

Each house would have a parking space and a bin store at the rear with access off the back street for two of the properties and off Gladstone Street for the end house. There would be small gardens at the front and rear.

Relevant Planning History

60869 - Erection of 4 no. dwellings - Withdrawn by Applicant 20/01/2017

Publicity

The following neighbours were notified by letter dated 24/02/17. 1-45 Shaw Street, 1-25 (odd) Maxwell Street, 2-18 (even) Gladstone Street,

One representation received from the resident at 7 Maxwell Street, situated to the rear of the site.

- The proposal would restrict access to my garage due to the distance between fence & my garage.
- Trees which were cut down prior to the purchase of the land and before the planning & again on the application has indicated there are no trees. The trees were one of the main reasons I purchased my property as it could be an area to be enjoyed by local residents & a safe place for children to play on.
- This application in my opinion has already destroyed part of our local environmental enjoyment & the land should be returned to its previous condition.

The objector has been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection.

Drainage Section - No objection.

Environmental Health - No objection.

United Utilities - No objection.

Unitary Development Plan and Policies

EN1/2 Townscape and Built Design

H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
EN5/1	New Development and Flood Risk
EN7	Pollution Control
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury
SPD16	Design and Layout of New Development in Bury
NPPF	National Planning Policy Framework
HT1	A Balanced Transportation Strategy
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
EN7/5	Waste Water Management

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - UDP Policy H1/2 - Further Housing Development states that the Council will have regard to various factors when determining a proposal for residential development including the availability of infrastructure, the suitability of the site, the nature of the local environment and the surrounding land uses.

The application site and surroundings are within the urban area and is residential in character. As such the proposed development would be appropriate in land use terms and would not conflict with the surrounding uses. Furthermore it is considered that there is adequate infrastructure to support the development. Subject to compliance with other policy considerations, the proposed development would be in accordance with Policy H1/2 Further Housing Development.

Design and Siting - Given the predominant residential street pattern, traditionally designed terraced houses would be suitable on the site. The houses, in line with the existing row of terraces and at the same height, appear to sit comfortably on the street. The development of three houses would not appear out of character within the streetscape and represents a good use of the site. Given the limited size of the plots, it is considered appropriate to restrict the 'permitted development' by a suitable condition.

The proposal is considered to be acceptable and complies with the NPPF and UDP Policies EN1/2, H2/1 and H2/2 with regard to new housing development and associated guidance.

Residential Amenity – The separation distances across Shaw Street (approx 18m) to the front and the back street (18-20m) to the rear are a little less than the minimum 20m required by existing guidance, but this is generally the case for the majority of the houses along the street. Given the existing street pattern and the existing separation distances between neighbours, it would be reasonable to allow some degree of flexibility in terms of 'window to window' distances and it is noted that there are no objections from the immediate neighbours in this regard. It is also important to note that the adopted guidance in SPD6 Alterations and Extensions indicates that reduced separation distances are acceptable where there is a fixed street pattern as in this case. With regard to residential amenity, the proposal is considered acceptable and compliant with Policy H2/1 The Form of New Residential Development and SPD 6 Alterations and Extensions.

Traffic - The proposal has been revised to address some minor concerns of the Traffic Team and is now considered to be acceptable subject to conditions. The on-site parking for a single car on each plot with access onto the back street is acceptable in this area and complies with UDP Policy HT2/6 Car parking and New Development.

Refuse - Each house would have a bin store in the rear garden with access onto the back street for collection. This is acceptable and complies with Policy H2/2 The Layout of New Residential Development.

Objection - As the proposed new rear boundary wall would be built at the back of the highway with no garage doors opening over the highway at the rear, there should be no obstruction to access from existing properties to the rear.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to Survey dated Oct 2016 and drawings numbered RAD/1522/16/1/RevA, RAD/1522/16/2/RevA, RAD/1522/16/3/RevA, RAD/1522/16/4/RevA and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details of all external finishing materials to be used in elevations, boundary walls and doors/gates and areas of hardstanding, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 Townscape and Built Design.
4. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory

development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework and UDP Policy 7 Pollution Control.

5. Following the provisions of Condition 4 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

6. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 of Schedule 2 of the Order, without the submission and approval of a relevant planning application.
Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.

7. Development shall not commence until details of foul and surface water drainage aspects have been submitted to and approved in writing by the Local Planning Authority. This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution.

Reason. Insufficient details have been provided and to ensure the proposed drainage scheme is appropriate and complies with UDP Policies EN5/1 New Development and Flood Risk and EN7/5 Waste Water Management.

8. Notwithstanding the details indicated on the approved plans, no development shall commence unless and until full details of the following have been submitted to and agreed with the Local Planning Authority:
 - Formation of the proposed pedestrian accesses onto Shaw Street and footway crossings onto Back Shaw Street , incorporating the provision of inward opening pedestrian gates, refurbishment of all footways abutting the site affected by the proposed development and statutory undertakers connections to the proposed dwellings to a scope and specification to be agreed and all associated highway and highway drainage remedial works;
 - Amendments to the proposed boundary treatment at the junction of Back Shaw Street with Gladstone Street in order to adequately accommodate the plotted visibility play;
 - All boundary walls abutting the adopted highway incorporating foundations that do not encroach under the adjacent adopted highway;
 - Roller shutter doors for each plot incorporating mechanism boxes/enclosure that do not project over the adjacent adopted highway on Back Shaw Street;
 - Revised elevation details confirming the provision of a roller shutter door and access position onto Back Shaw Street to the plot abutting Gladstone Street
 - Relocation on land within the applicant's control of the Royal Mail pouch box on the Shaw Street frontage affected by the proposed development.

The details subsequently approved shall be implemented to an approved programme and maintained thereafter.

Reason. Details have not been submitted and to secure the satisfactory development of the site in terms of highway safety, ensure good highway design and maintain the integrity of the adopted highway pursuant to UDP Policy H2 Highway Network .

9. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:
- Hours of operation and number of vehicle movements;
 - Parking on site of operatives' and construction vehicles together with storage on site of construction materials.
- The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials.
- Reason. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety pursuant to UDP Policy H2 Highway Network.
10. No development shall commence unless and until details have been submitted to and approved by the Local Planning Authority to cover measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations. The approved details shall be implemented and maintained thereafter during the period of demolition/construction.
- Reason. To ensure that the adopted highways are kept free of deposited material from the ground works operations.
11. The visibility splays indicated on the approved plans shall be implemented to the written satisfaction of the Local Planning Authority before the development is first occupied and subsequently maintained free of obstruction above the height of 0.6m.
- Reason. To ensure the intervisibility of the users of the site and the adjacent highways in the interests of road safety pursuant to UDP Policy HT2 Highway Network.
12. The car parking arrangements indicated on the approved plans incorporating 5.2m minimum length hardstandings and openings fitted with roller shutter doors or a similar approved type which do not project outwards at any time during or after operation shall be made available for use to the written satisfaction of the Local Planning Authority prior to the dwellings hereby approved being occupied and thereafter maintained at all times.
- Reason. To ensure adequate off street car parking provision and compliance with Section 153 of the Highways Act 1980 which prohibits the opening of doors, gates or bars outwards onto the street, in the interests of highway safety.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 61163

ADDRESS: Land at Shaw Street/Gladstone Street
Bury

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

61163

Photo 1



Photo 2



Photo 3



Photo 4





Date	Rev.		Initials
19/2/17	A.	Proposed revised to 3 dwellings	RA
23/4/17	B.	Gate accesses altered	RA
15/5/17	C.	Boundary walls altered	RA

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Figured dimensions take precedence over scaled measurements from the drawing. All dimensions to be checked by Contractor and discrepancies clarified with the Client prior to commencement.
The whole of the works to comply with current Building Regulations. No work to commence without Building Regulations approval.
The client must ensure the project complies with the Construction Design and Management Regulations 2015

Notes

RAD

The White House,
42-44 Chorley New Road,
Bolton
BL1 4AP

Tel : 01204 322196
Fax : 01204 214110
Mob: 07790 361104
Email: info@radesignprojects.co.uk

Project

LAND AT SHAW STREET
BURY
BL9 7QD

Title

PROPOSED SITE PLAN

Client

RA

Date

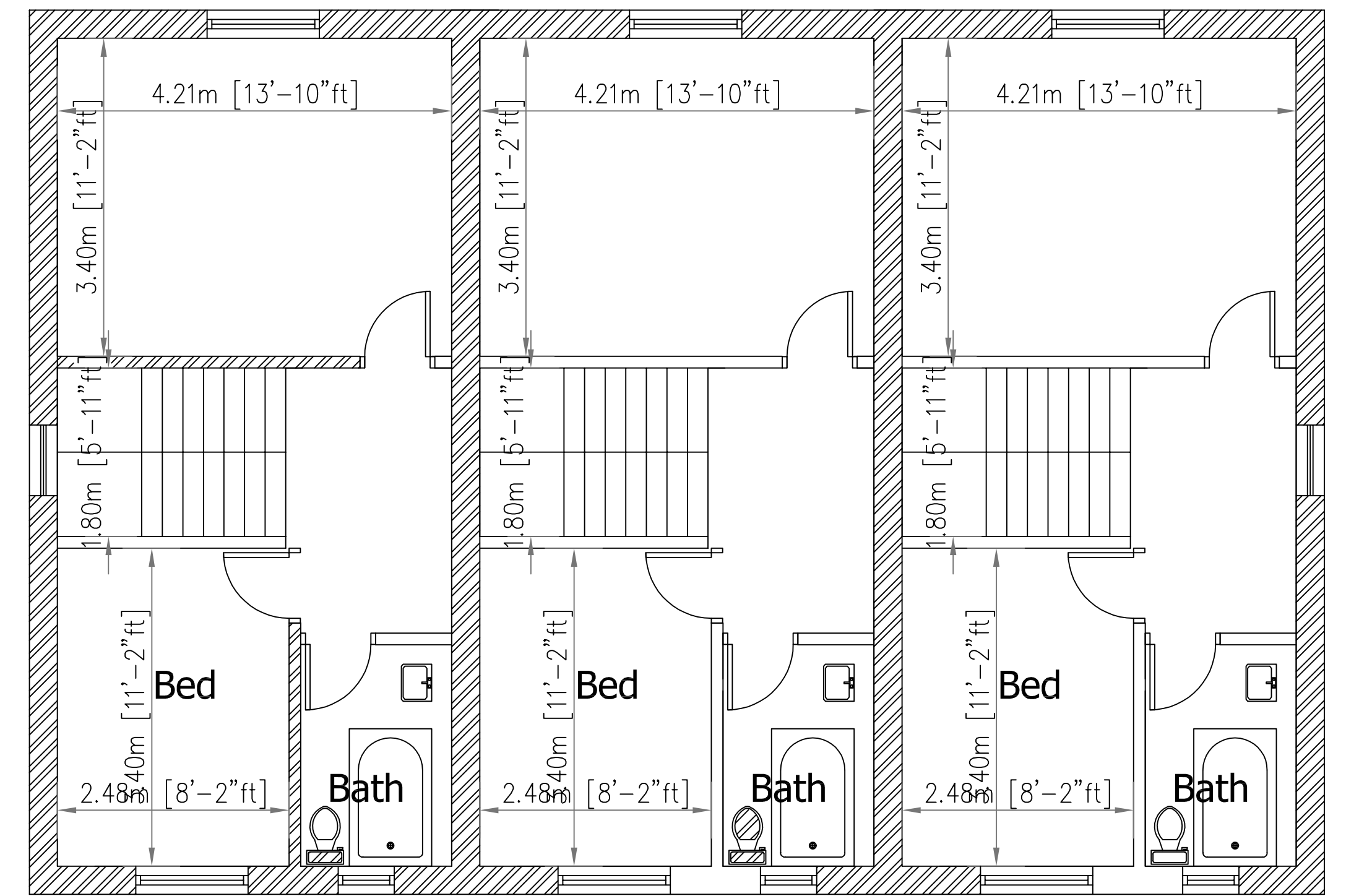
NOVEMBER 2016

Scale

1:50 @ A1

Drawing Number

RAD/1522/16/4/RevC



PROPOSED FIRST FLOOR

Date	Rev.		Initials
19/2/17	A.	Proposed revised to 3 dwellings	RA
23/4/17	B.	Gate accesses altered	RA
15/5/17	C.	Boundary walls altered	RA

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The client must ensure the project complies with the Construction Design and Management Regulations 2015

Notes

RAD

The White House,
42-44 Chorley New Road,
Bolton
BL1 4AP

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Fax : 01204 214110
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Email: info@radesignprojects.co.uk

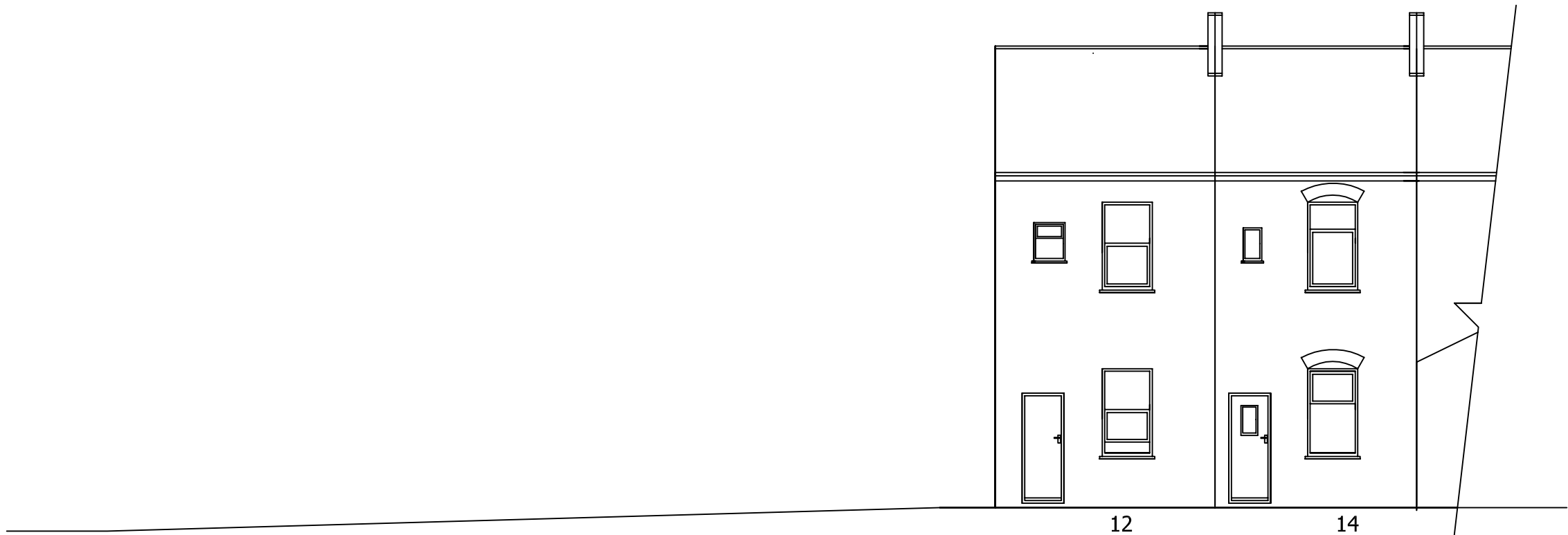
Project
LAND AT SHAW STREET
BURY
BL9 7QD

Title
PROPOSED FLOOR PLANS

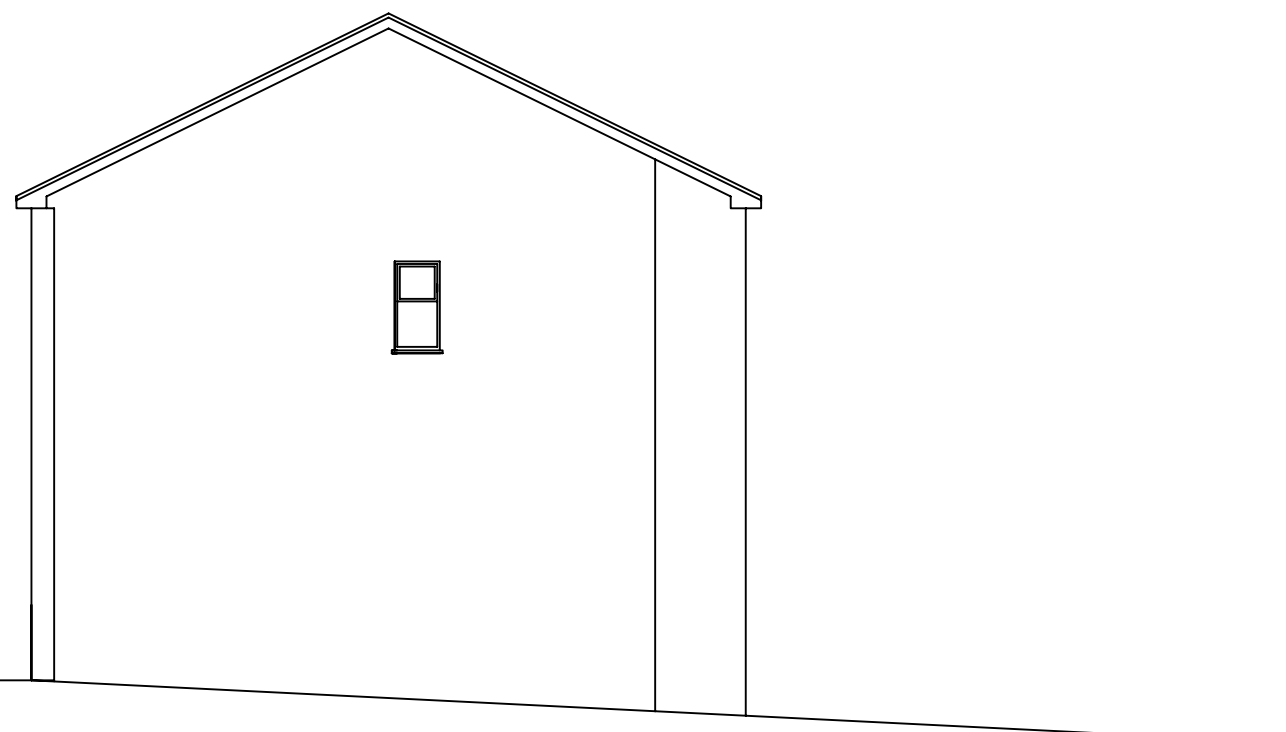
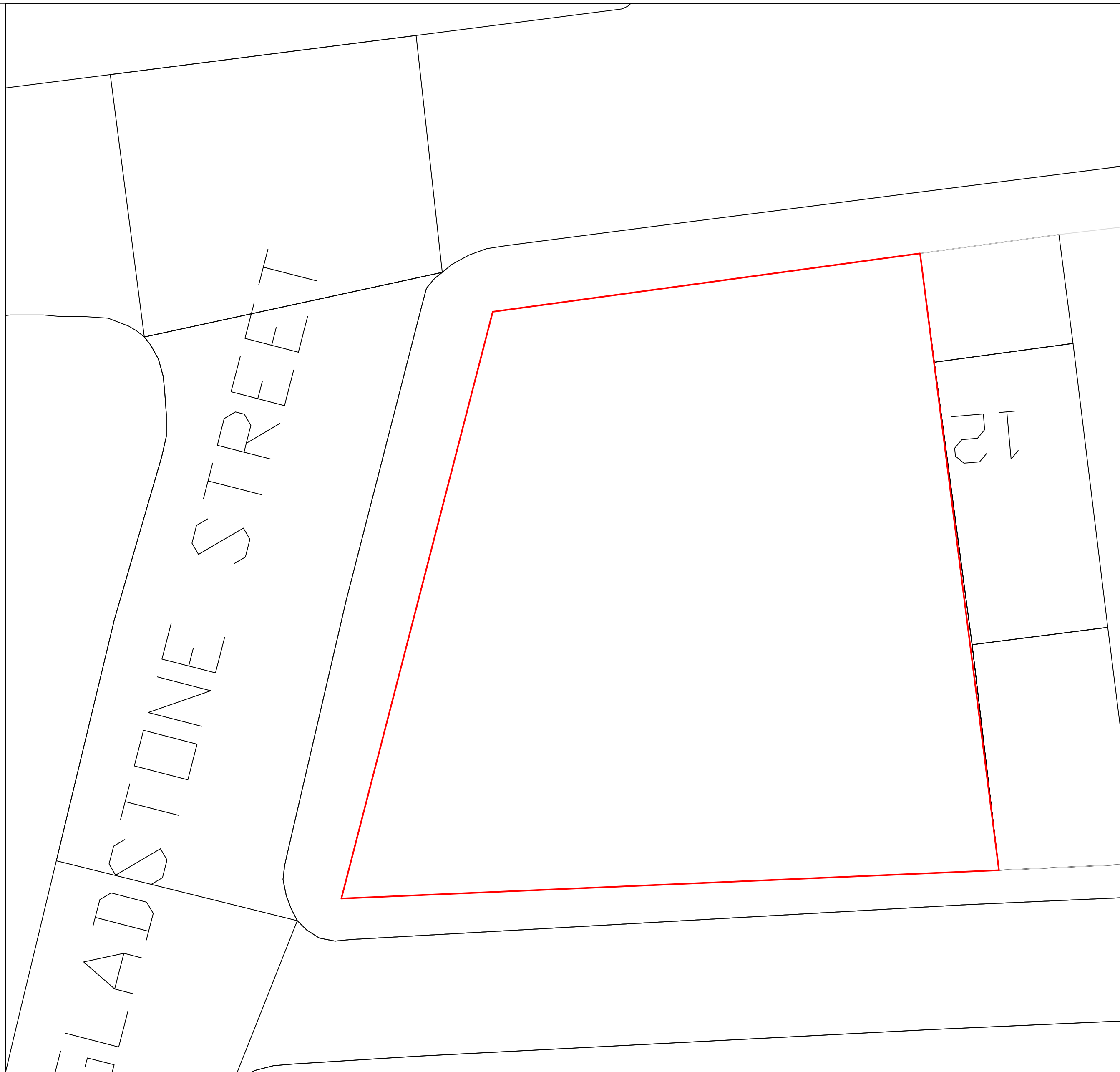
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Drawn RA
Date NOVEMBER 2016
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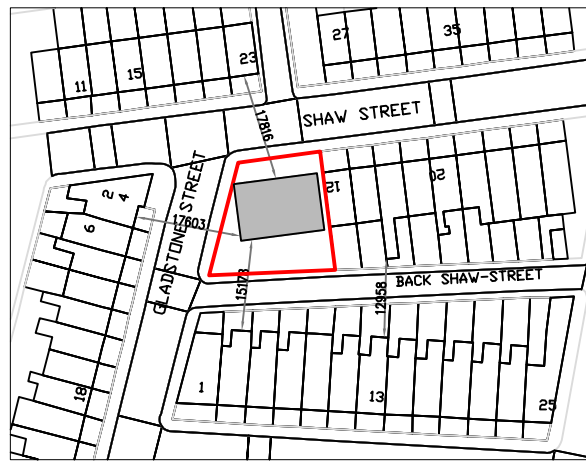
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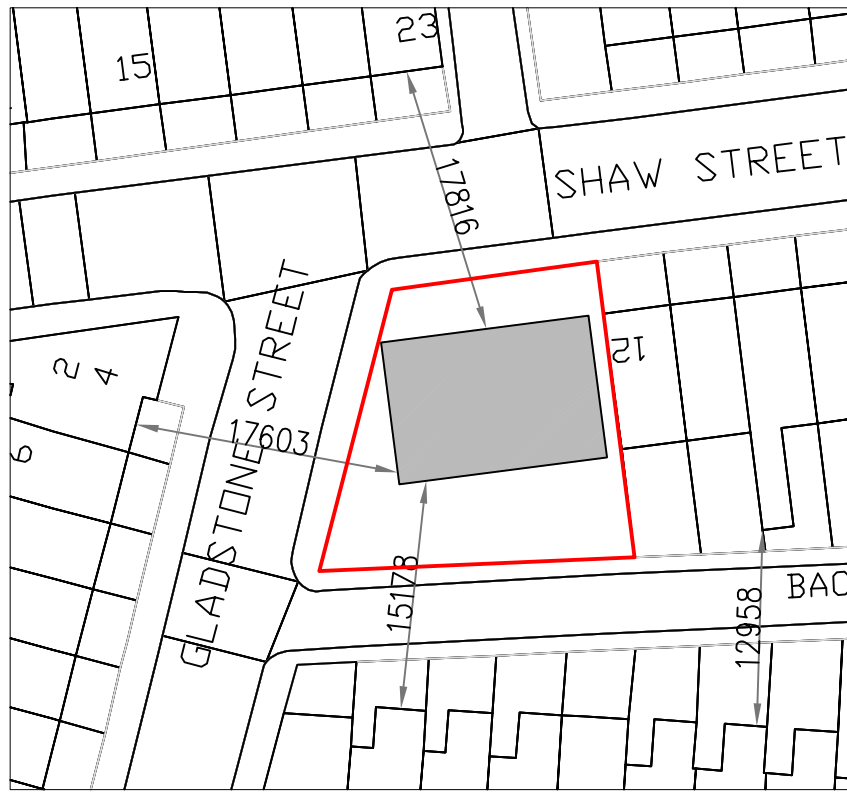
EXISTING REAR ELEVATION (REAR SHAW STREET)



EXISTING SIDE ELEVATION (12 SHAW STREET)



LOCATION PLAN (1:1250)
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Ordnance Survey 0100031673



BLOCK PLAN (1:500)

Date	Rev.		Initials
19/2/17	A.	Proposed revised to 3 dwellings	RA

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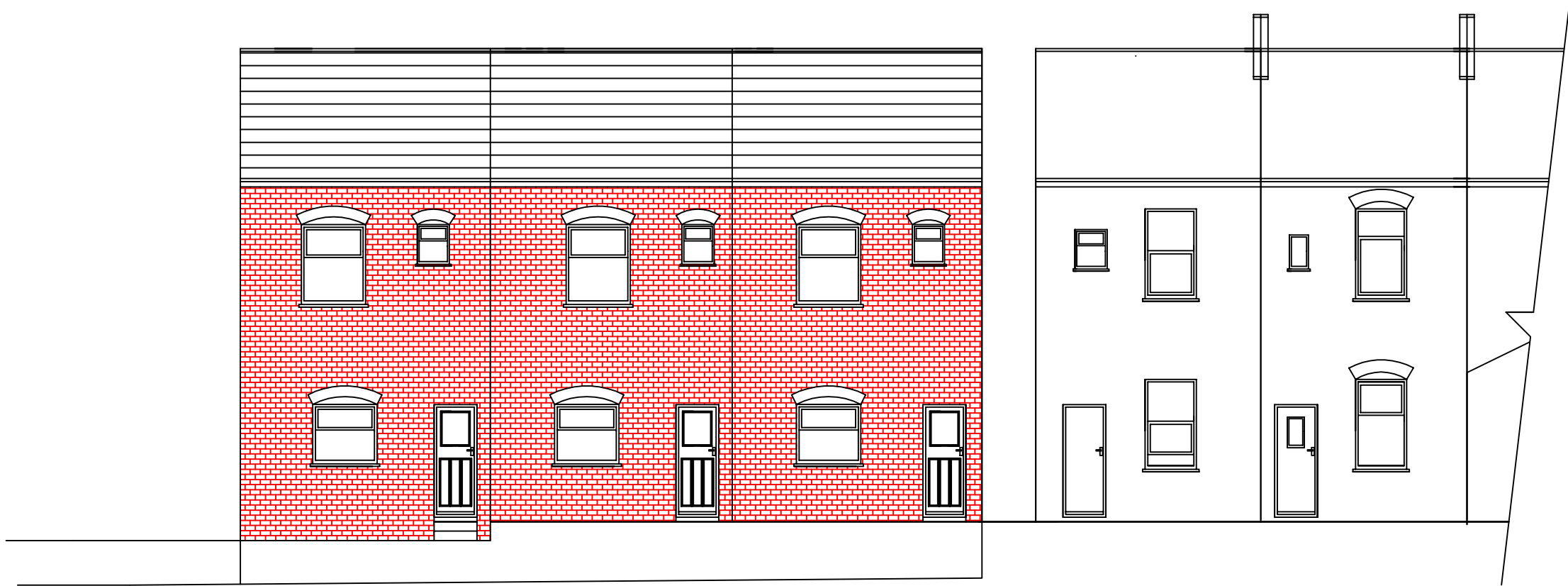
Project
LAND AT SHAW STREET
BURY
BL9 7QD

Title
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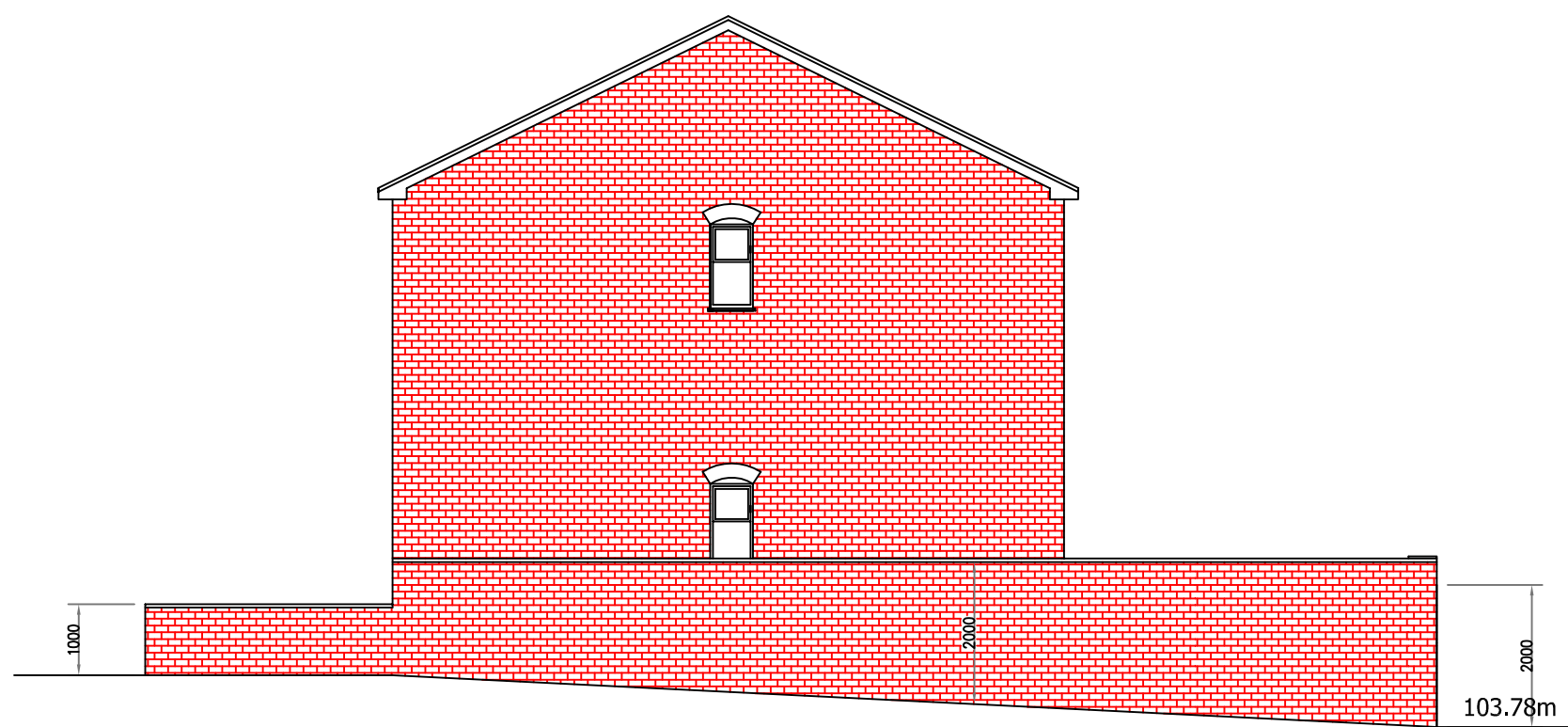
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Drawn RA
Date NOVEMBER 2016
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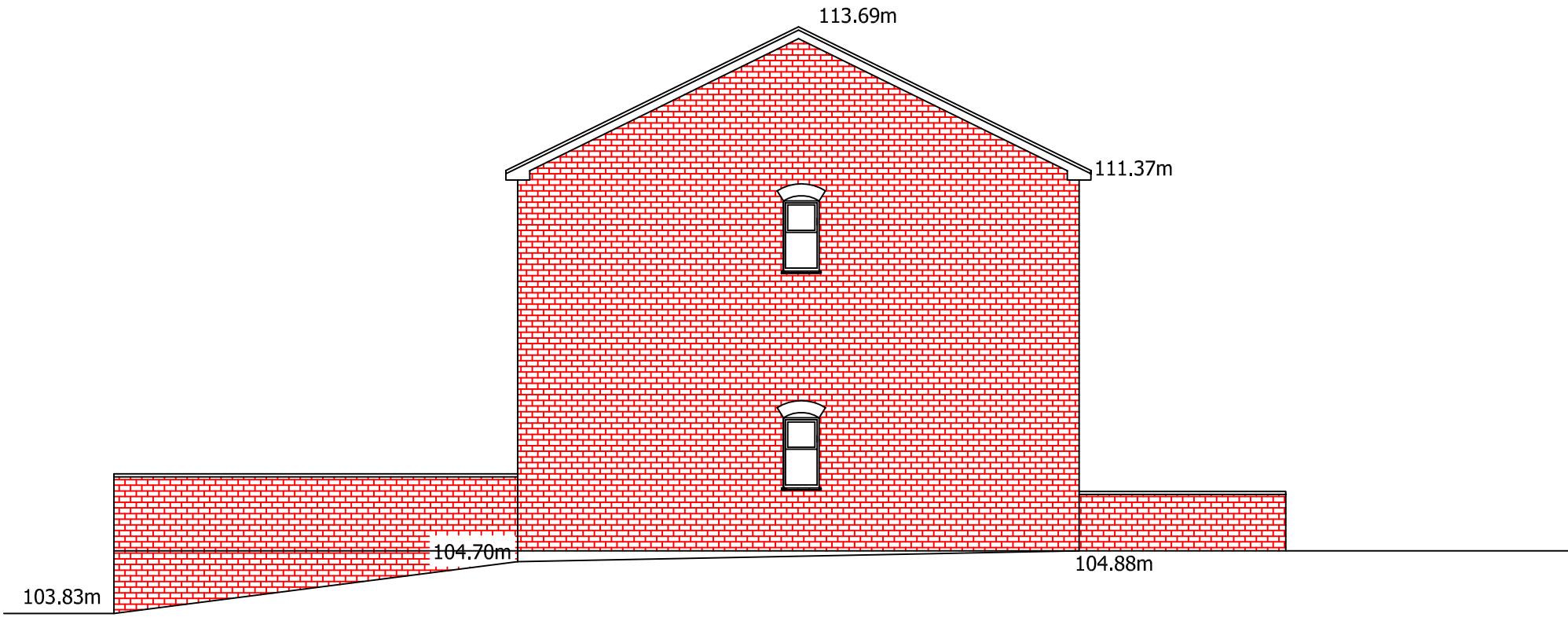
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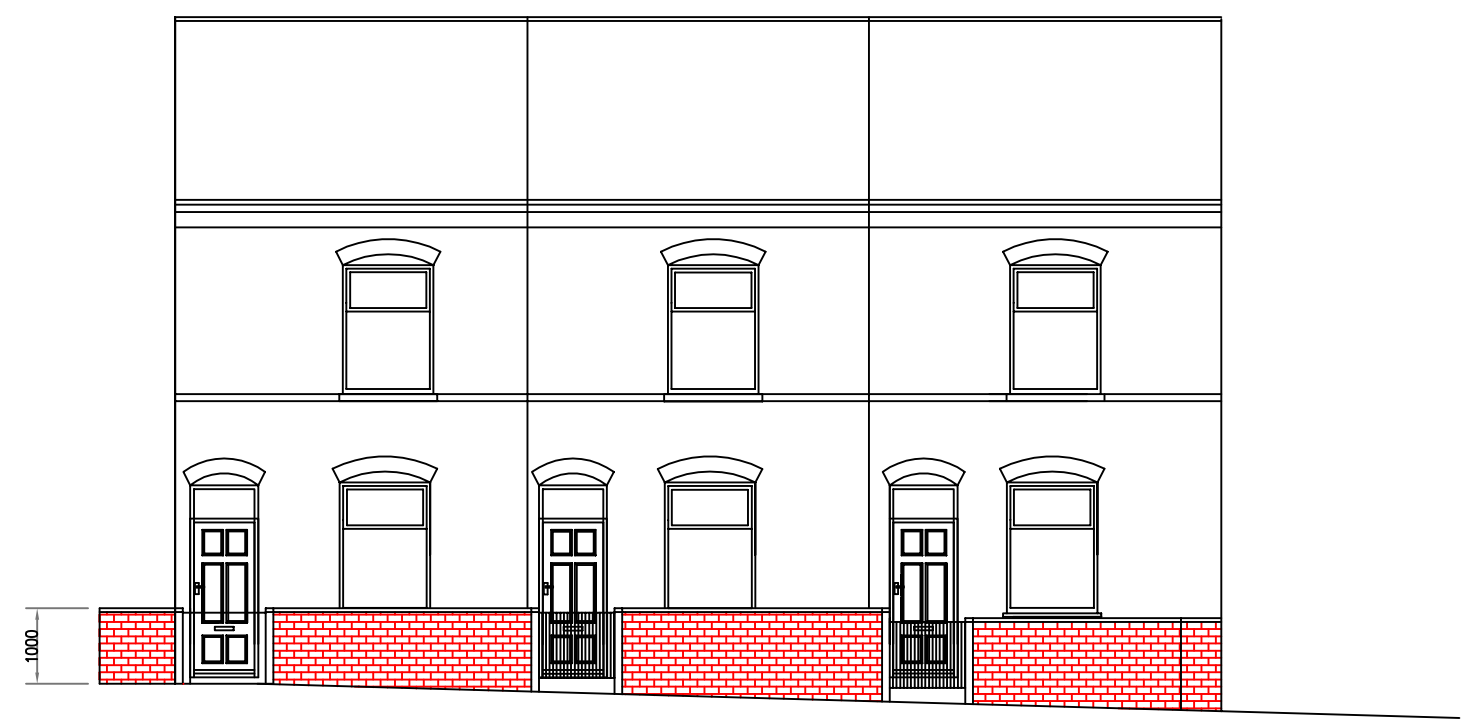
PROPOSED REAR ELEVATION (REAR SHAW STREET)



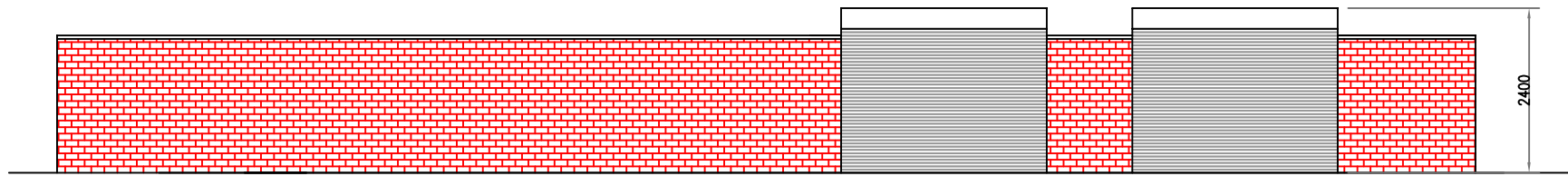
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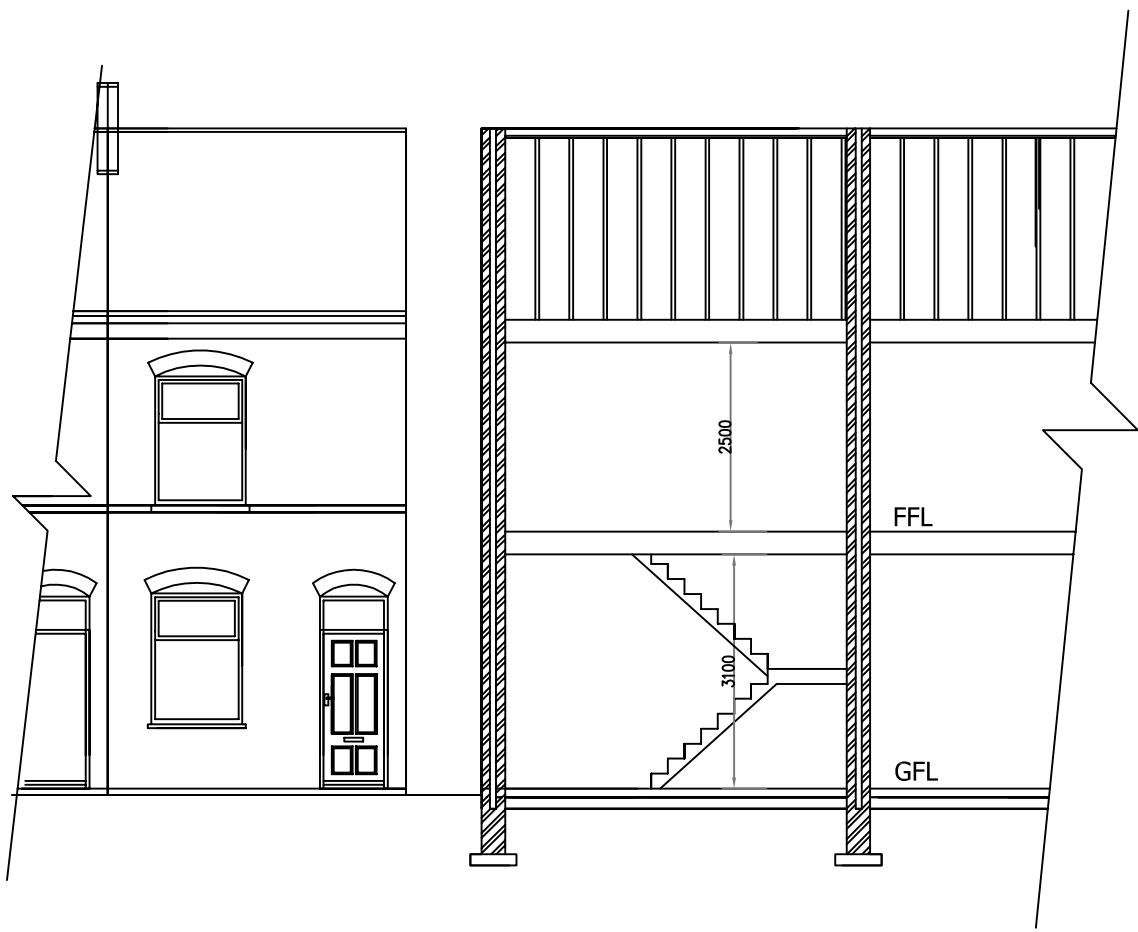
PROPOSED SIDE ELEVATION



PROPOSED FRONT BOUNDARY WALL



PROPOSED REAR BOUNDARY WALL



SECTION

Date	Rev.		Initials
19/2/17	A.	Proposed revised to 3 dwellings	RA
23/4/17	B.	Gate accesses altered	RA

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Project
LAND AT SHAW STREET
BURY
BL9 7QD

Title
PROPOSED SECTION & ELEVATIONS

Client
Drawn RA
Date NOVEMBER 2016
Scale 1:100
Drawing Number RAD/1522/16/2/RevB

Ward: Ramsbottom and Tottington -
Ramsbottom

Item 04

Applicant: Miss Christine Heald

Location: Sheep Hey Farm House, Leaches Road, Shuttleworth, Ramsbottom, Bury, BL0 0ND

Proposal: Retention of shed to store hay, feed and associated equipment; retention of boundary fencing

Application Ref: 61223/Full

Target Date: 19/05/2017

Recommendation: Approve with Conditions

Description

Sheep Hey is a small development of 8 dwellings situated within the Green Belt, surrounded by open fields. Access to the development is along Leaches Road which is a small track accessed from Whalley Road. On approaching the development there is a large field which is used as a camp site. Numbers 3 to 8 Sheep Hey are two storey stone built properties which front on to a communal courtyard to the front of the properties with private garden areas to the rear. Number 3 Sheep Hey has a drive way with car port at the side of the house and there are garages to the south of the properties belonging to numbers 4 to 8 Sheep Hey.

Kays Cottage (2 Sheep Hey) is attached to the north east side of the application property. This property has several out buildings to the north of the property and they own the field/camp site to the south of the residential properties at Sheep Hey.

The application property is a semi detached stone built farm house with gardens to three sides. To the west and to part of the southern boundary there are large mature trees along the boundaries. There is a stone wall with fence along part of the southern boundary and a large detached stone garage to the south east of the site which is currently being repaired. Across the lane to the south of the residential site is a small paddock belonging to the applicant which has a stable block, hardstanding, shed and small allotment. Beyond the paddock are fields which also belong to the applicant.

This application consists of two parts. The first part of the application is for the retention of a fence which the applicant advises was erected in 2012 along the southern side of the residential curtilage. However, there is no Certificate of Lawful Development to confirm this. The fence has been erected on top of the original stone boundary wall to an overall height of 1.95m from ground level. They advise the fence was erected because they have three large dogs and as this section of the boundary runs alongside a public bridleway/footpath the fence prevents the dogs from being disturbed by passers by, walkers and other animals.

The second part of the proposal is for the retention of a shed built approximately three years ago. The shed is situated across the lane to the south of site on a small paddock. The structure is an L-shaped building built from wooden panels with a felt roof. It is similar in appearance to the existing stable block on the site. It measures 3.1m to the ridge, 2.6m to the eaves and a total width of 5.5m. The main part of the shed is used for the storage of hay, feed etc and other associate articles relating to the keeping of horses and sheep (for domestic use), as well as gardening equipment in respect of the vegetable patches to the south east of the building. This structure is a building which is not in the residential curtilage and as such requires planning permission.

Relevant Planning History

01937/E - Pre-application enquiry for the retrospective erection of shed and fencing -
Enquiry completed 31/01/2017

16/0475 - Construction of a building and High Fence - 24/03/2017

Publicity

Neighbours at numbers 2, 3, 4, 5, 6, 7 and 8 Sheep Hey were notified about the proposal on 24.03.17. As a result of this publicity one objection has been received from the neighbour at Kays Cottage, 2 Sheep Hey attached to the property on site. Points raised include;

- The property is in the Green Belt. They had great difficulty in gaining planning permission for our recent application for facilities associated with camping. My neighbour's planning application also ought to be dealt with in accordance with Green Belt Policy.
- The shed is supposed to be for the storage of hay, feed and associated articles for the keeping of horses and sheep. There are no sheep on the land and only two horses. The existing stable building should be more than adequate for all their requirements. The applicant has established an unauthorised garden with what is simply and over large and excessively high garden shed.
- Both the change of use of the land to domestic garden and the shed are inappropriate development in the Green Belt. The shed in particular, harms the openness of the Green Belt. There has been no attempt to demonstrate "very special circumstances" and there is no justification for granting planning permission.
- The whole of the fence is not edged red and does not appear to be included within the planning application even though it abuts the access road to my house and other properties at Sheephey. This needs to be clarified. I do not think that the fence was erected as long ago as 2012 and, without incontrovertible evidence, it has not therefore become "lawful" as the applicant claims. In my opinion the fence is unnecessarily high and detracts from the appearance of both our house and the applicant's.

The objector has been informed of the Planning Control Committee meeting.

Consultations

None necessary

Unitary Development Plan and Policies

OL1	Green Belt
OL5/2	Development in River Valleys
OL1/2	New Buildings in the Green Belt
EN1/2	Townscape and Built Design
EN6/4	Wildlife Links and Corridors
EN9/1	Special Landscape Areas
SPD6	Supplementary Planning Document 6: Alterations & Extensions
H2/3	Extensions and Alterations
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

The main considerations of this application are the impact of the proposal on visual and residential amenity.

Principle - Paragraph 89 of the NPPF states that 'A Local planning Authority should regard the construction of new buildings as inappropriate in the Green Belt. Exceptions to this are:

- buildings for agriculture and forestry;
- provision of appropriate facilities for outdoor sport, outdoor recreation and for cemeteries, as long as it preserves the openness of the Green Belt and does not conflict with the purposes of including land within it;
- the extension or alteration of a building provided that it does not result in disproportionate additions over and above the size of the original building;
- the replacement of a building, provided the new building is in the same use and not materially larger than the one it replaces;
- limited infilling in villages and limited affordable housing for local community needs under policies set out in the local plan; or
- limited infilling or the partial redevelopment of previously developed sites, whether redundant or in continuing use, which would not have a greater impact upon the openness of the Green Belt and the purpose of including land within it than the existing development.

Policy OL1/2 states that the construction of new buildings within the Green Belt will be inappropriate development unless it is for agriculture, forestry, essential facilities for outdoor recreation, limited extensions of existing dwellings and for other uses of land which do not conflict with the purposes of including land in it. Proposals for buildings, which do not fall into one of the above categories is inappropriate development and will only be permitted in special circumstances.

The development includes a timber constructed shed which could be considered as a non-permanent structure which could be removed at any given time. The structure is similar in appearance to the stable building on the same land and would be used for the storage of equipment/goods in relation to outdoor recreation (animal feed, hay and associated articles for the keeping of horses and domestic sheep).

The fence along the side boundary of the residential curtilage to the north of the paddock has also been constructed with timber which is similar in appearance to that of the shed and the short timber fence surrounding the paddock. With this in mind both the shed and fence are considered to be acceptable in principle with both Bury UDP Policy OL1/2 and Paragraph 89 of the NPPF subject to the detailed assessment below.

Green Belt and Visual Amenity - Sheep Hey comprises of eight residential dwellings within the Green Belt with non-residential land adjacent belonging to the owners of 1 and 2 Sheep Hey.

The fence which has been erected on top of the original boundary wall of the residential curtilage has been constructed with timber vertical panels which is similar to the materials of other buildings in the nearby vicinity and is considered to maintain the character of the countryside setting. The height of 1.95m is not considered excessive and the materials used are not unusual in respect of fences nor in a rural setting. The design of the proposal is considered acceptable in this location. As the curtilage of the property has not been extended the fence does not have any impact on the openness of the Green Belt.

With regards to the shed which has been erected, it is located on non-residential land opposite the property this is positioned within a small compound alongside an existing stable building. Its timber construction is similar in appearance to the original stables on the site and approximately the same height as the stables opposite the front of the shed. It is a relatively small structure adjacent to the stables and of a timber construction which would be expected in this type of environment. It is not considered to be out of character in the countryside setting. It is to be used for animal feed and horse and gardening equipment which would also not be considered unusual at this type of domestic property.

With this in mind and given the height of the shed and its position within a cluster of other

buildings it does not have any impact on the openness of the Green Belt or the existing street scene and therefore complies with the NPPF and UDP Policies OL1 - Green Belt and OL1/2 - New Buildings in the Green Belt as well as Bury UDP Policy H2/3 - Extensions and Alterations and the guidance in Supplementary Planning Document 6 – Alterations and Extensions to Residential Properties in terms of visual amenity.

Residential Amenity - The fence in question has been erected along a straight section of public footpath/bridleway with no overhanging planting or trees. It is over 11m from the dwelling and there is an existing stone detached garage on the same boundary. With this in mind the fence does not hinder any view points at the junction at the top of the bridleway close to the houses in the area as such the additional height of the fence on top of the original wall does not have any detrimental effect on people, animals or vehicles using the bridleway or the adjoining neighbour.

The shed has been erected wholly on land belonging to the applicant. It is not for domestic use only and houses hay, animal feed and equipment only which would be used in conjunction with the existing stables on the same land as the shed. It is set well back from the junction to the road in front and does not hinder any views of the road in front for users of the bridleway to the side. The proposal conforms to UDP Policies EN1/2 - Townscape and Built Design and H2/3 - Extensions and Alterations and the requirements of Supplementary Planning Document 6 – Alterations and Extensions to Residential Properties in terms of separation distance to the surrounding properties.

Response to neighbour objection -

- Issues concerning the new building in the Green Belt and the fence erected along the southern boundary of the site have been addressed in the Green Belt and Residential Amenity Sections of this report.
- In respect of the point made that the applicant has changed the used of the paddock and established an unauthorised garden and the application Google Maps shows that the area where the shed has been erected was allotments in 2009 and it has the same use now with the addition of the shed which is part of this application. It does not form part of the residential garden area.
- The reason why only one side section of the boundary fence has been included as part of this application is because only this section abuts a public highway.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

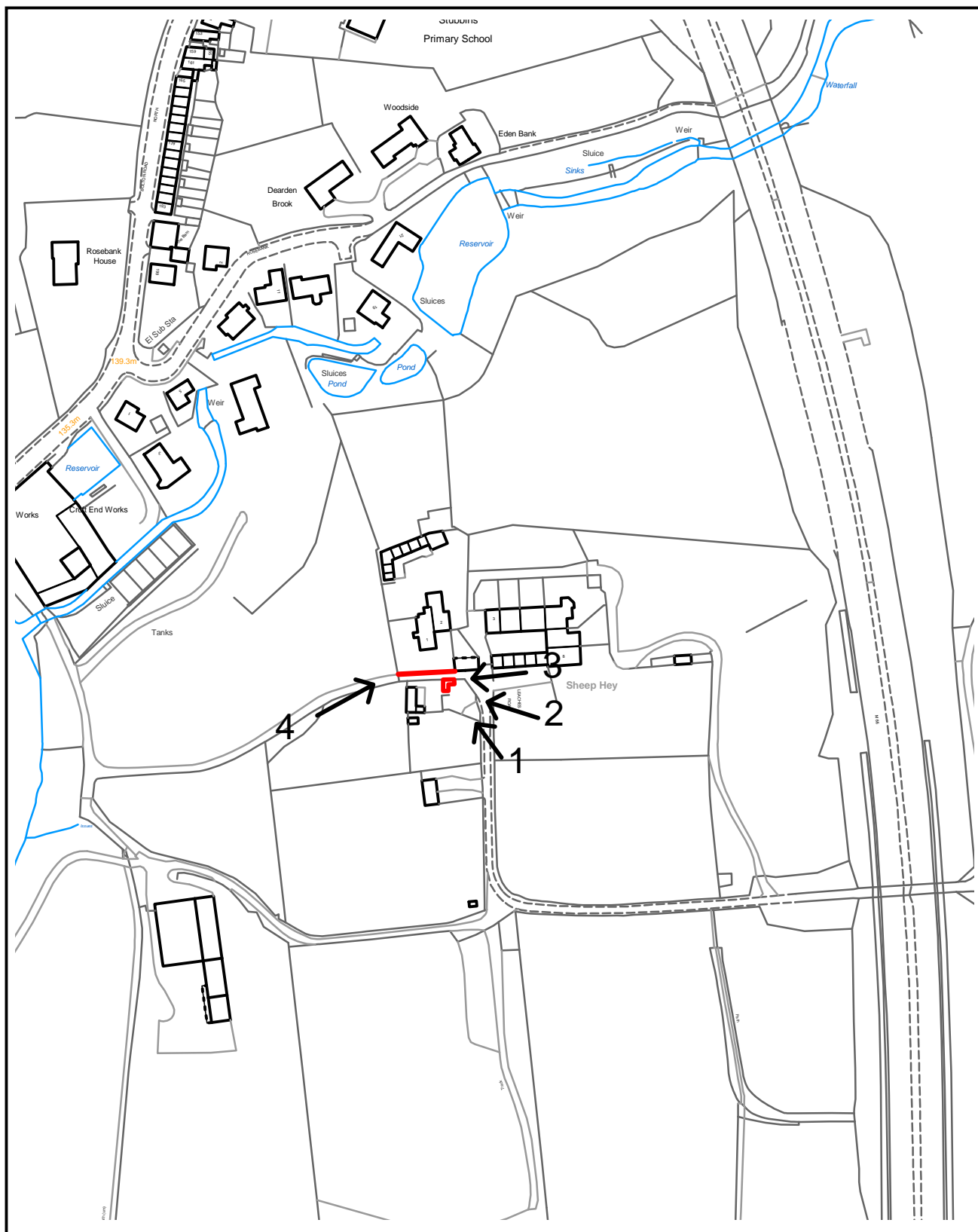
Recommendation: Approve with Conditions

Conditions/ Reasons

1. This decision relates to the drawings received on 06/03/17 and the development shall not be carried out except in accordance with the drawings hereby approved. Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury UDP Policy H2/3 - Extensions and Alterations and the guidance in Supplementary Planning Document 6 – Alterations and Extensions to Residential Properties.

For further information on the application please contact **Amanda Uhunmwagho** on **0161 253 5323**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 61223

**ADDRESS: Sheep Hey Farm House
Leaches Road
Shuttleworth**

Planning, Environmental and Regulatory Services



Bury
COUNCIL

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61223

Photo 1



Photo 2

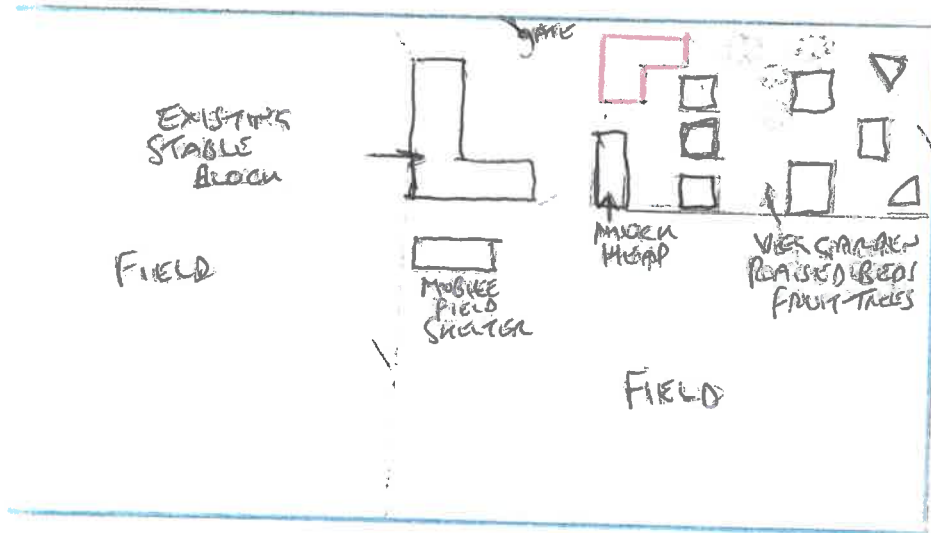
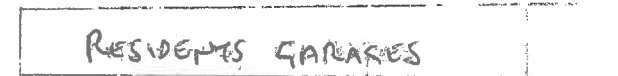
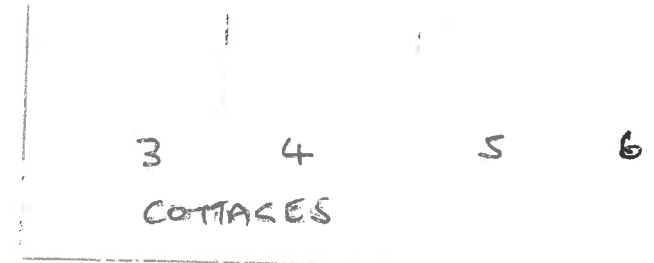
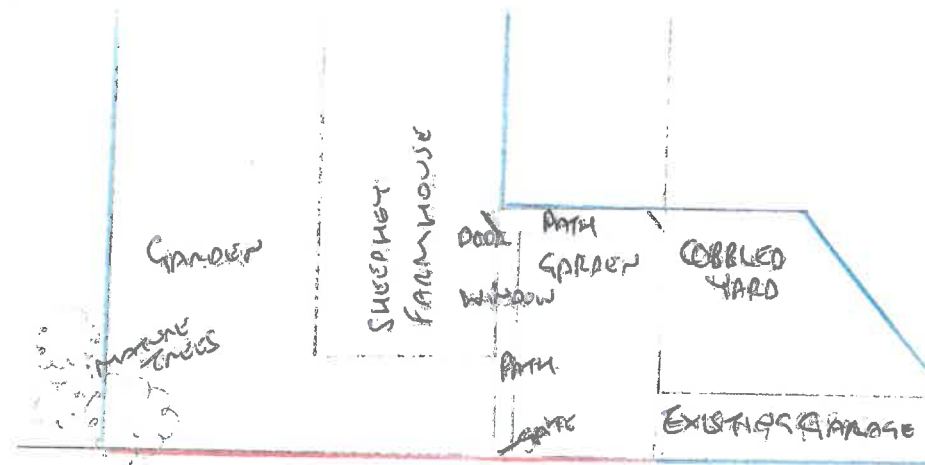


Photo 3



Photo 4

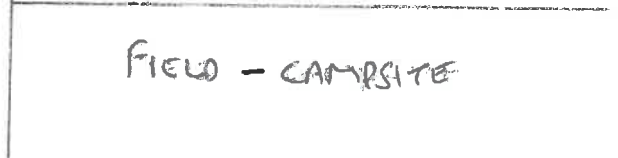


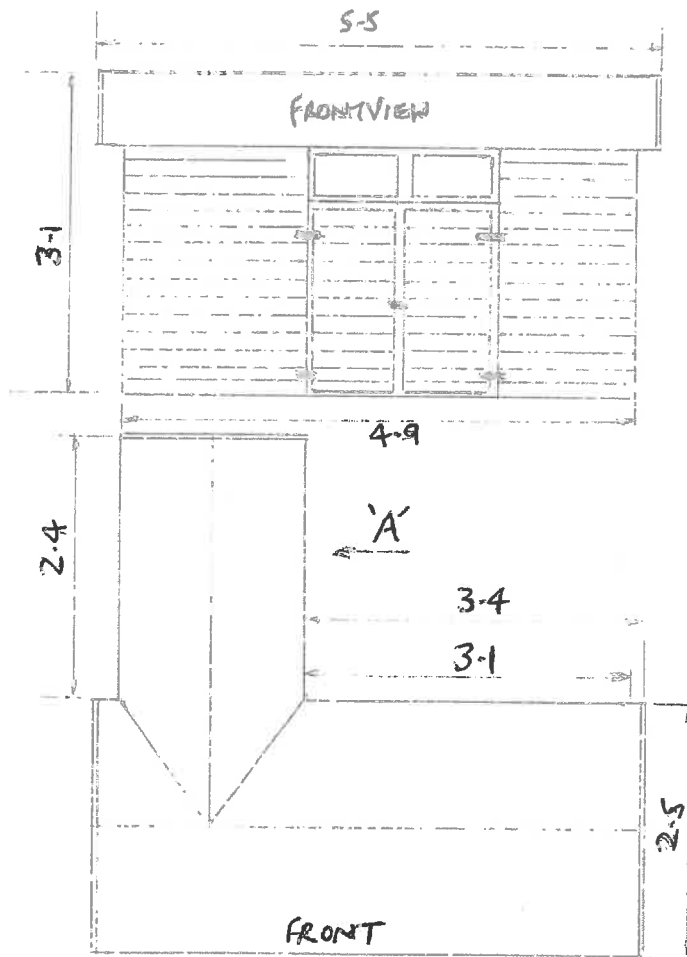


LEACHES ROAD

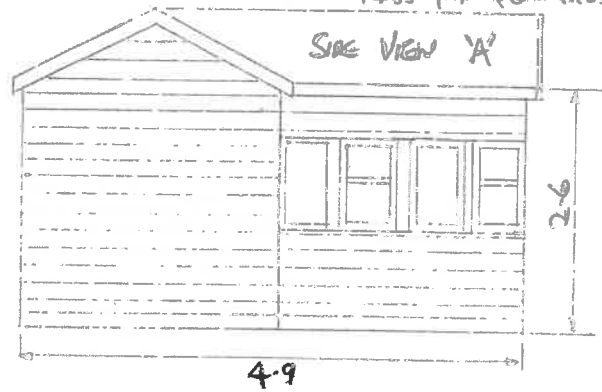


7/3/17

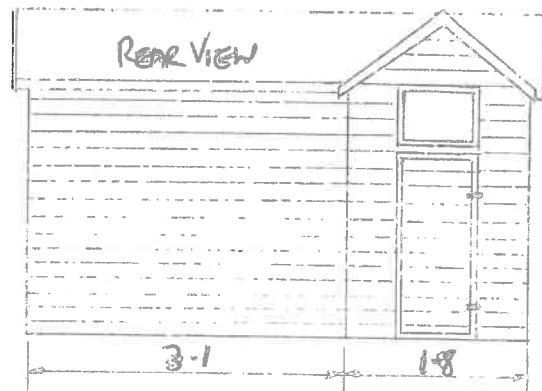


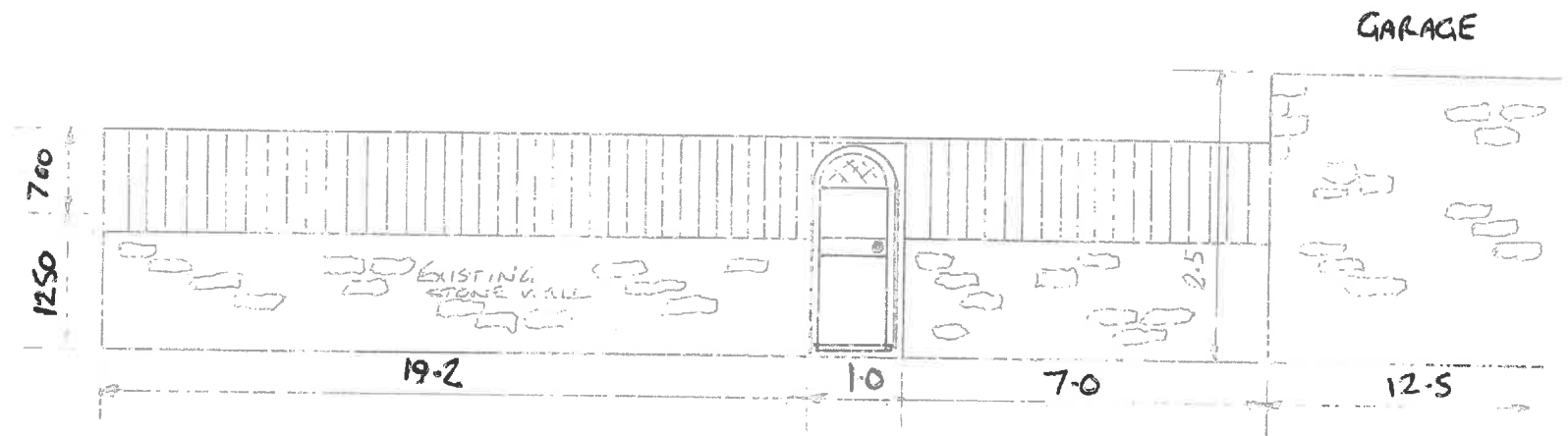


MATERIALS - TIMBER FRAME COVERED WITH
125% WEATHER BOARD
ROOF: DUTCH GABLE RED OF 15 FOOT
GLASS 4MM TONED



BASE - CONCRETE STONE





MATERIALS - TIMBER CLADDING (CEILING)
 WITH 12.5" WEATHER BOARD
 LEAK PROOF FLOOR BOARD

Ward: Bury West - Church

Item 05

Applicant: Mr Karl Rawston

Location: Land at 4 Mile Lane, Bury, BL8 2DS

Proposal: Erection of 1 no. dwelling at side

Application Ref: 61286/Full

Target Date: 17/05/2017

Recommendation: Approve with Conditions

Description

The application site relates to the side and part of the rear garden of No 4 Mile Lane. This property is a 2 storey brick built detached dwelling and has an in/out access to a driveway and an integral garage on the northern side of the house. The property has a large rear garden which currently accommodates a summerhouse in the southern eastern corner.

The site is bounded by a dwarf wall at the frontage, and there are mature conifer trees and fence to No 2 Mile Lane, a bungalow which is set at a lower level and forward of the building line of No 4 Mile Lane.

To the rear are properties on Turton Close, separated by a boundary fence which are also set at a lower level of approximately 1m. No 14, a bungalow, is directly opposite the application plot and No 12 directly faces the rear elevation of No 4 Mile Lane.

The application is for a 2 storey detached dwelling with a rear dormer, to be sited in the side garden of No 4 Mile Lane. It would be set 1m from the side elevation of No 4 and forward of the front elevation of the house by 2m. There would be a distance of 6.8m to the boundary with No 2 Mile Lane.

The proposed dwelling would project 12m in length, 3.4m beyond the rear elevation of No 4. A 6.5m long rear garden would be provided bounded by a new fence. Beyond this, to the boundary of No 14 Turton Close, a remaining garden length of 5.5m would continue in the ownership of No 4 Mile Lane as access to their summerhouse which would be retained in the south eastern corner.

The proposed dwelling would share the existing central vehicular access with No 4 and there would be 2 parking spaces to the front of the new property.

In terms of appearance, the proposed dwelling would match the eaves level of No 4 with the roof slightly higher by approximately 0.65m. The dwelling would have a front pike roof detail and incorporate a dormer and bedroom/en suite in the roof space at the rear. Proposed materials are facing brickwork and concrete roof tiles.

Relevant Planning History

43593 - Residential development - two storey detached three bedroom dwelling - Refused 30/11/2004 - by reason of its size, position and design, to be a prominent and intrusive feature in the street scene and would therefore, be seriously detrimental to the visual amenities of the area (see response to objections in the section below).

48915 - Two storey rear extension & first floor side extension - Approve with Conditions 20/12/2007

52832 - Certificate of lawful development for proposed detached summerhouse at rear - Lawful Development 16/07/2010

55171 - New front boundary wall (Retrospective). - Approve with Conditions 22/06/2012

Publicity

12 letters sent on 29/3/2017 to properties at Nos 1,3,6 Standing Avenue, 12,14,16 Turton Close, 1,2,3,6,8 Mile Lane, 22 Freckleton Drive.

One letter of objection received from No 12 Turton Close (rear of No 4 Mile Lane) with the following issues raised:

- The dwelling would be 3 storey and 9m in height and the dormer would be an incongruous and dominating feature when viewed from my property and others at the rear;
- The dormer and its window position would allow occupants of the bedroom to look out and afford a clear, unobstructed view into properties at the rear;
- The recommended separation distances do not take into account that there is a change in levels of at least 1000mm between the site/No 4 Mile Lane and the houses at the rear which would increase the impact of any new structure. An increase in separation distance to allow for the difference in levels would lessen the impact;
- It would be possible to move the proposed dwelling forward to help address the separation distance concerns, and sensible to site between Nos 2 and 4 Mile Lane;
- Parking spaces would not be compromised by moving the new structure nearer to Mile Lane
- Request Permitted Development Rights are removed;
- The above suggestions should be incorporated but if not request the application be refused and considered by the Planning Committee.

Further objection received from No 12 Turton Close with detailed information dating back to 2003 of previous applications at the address of 4 Mile Lane which have either been refused, approved or withdrawn.

The objector states that even though time has moved on, the issues of these applications are the same as the current application.

Other comments made by the objector:

- As this application stands, the proposed house would have panoramic views of my living/social areas and outdoor space;
- I enjoy my privacy and time at home and am entitled to it. Including the difference in ground levels, the house would be over 2.5 times taller than mine and tower over it, so in real terms how could it not affect my privacy and other rights.

The objector has been informed of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection subject to conditions.

Drainage Section - Standard drainage condition to include a SuDS scheme will be required by condition.

Environmental Health Contaminated Land - No objection subject to conditions.

United Utilities (Water and Waste) - No objection subject to condition. The applicant should be aware of the location of a public sewer which crosses the site which will require an access strip of 6m.

The Coal Authority - No objection subject to a condition to carry out remedial measures prior to development.

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H2/6	Garden and Backland Development
EN1/2	Townscape and Built Design

HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
SPD6	Supplementary Planning Document 6: Alterations & Extensions
SPD11	Parking Standards in Bury

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Principle - Following revocation of the North West Regional Strategy on 20th May 2013, there is no statutory housing target for Bury. Work has commenced on the Greater Manchester Spatial Framework and this will bring forward a new statutory housing target for the Borough. This will subsequently be incorporated into Bury's future Local Plan.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable land.

UDP Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

UDP Policies H2/1 - The Form of New Residential Development and H2/2 - The Layout of New Residential Development takes into consideration factors relating to the height and roof type of adjacent buildings, the impact of developments on residential amenity, the density and character of the surrounding area and the position and proximity of neighbouring properties. Regard is also given to parking provision and access, landscaping and protection of trees/hedgerows and external areas.

UDP Policy H2/6 - Garden and Backland Development assesses proposals with special regard given to the concentration and relative density of such development in the surrounding area, impact on neighbouring properties and the local environment and access arrangements.

Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties provides useful guidance in terms of acceptable aspect standards between dwellings and design criteria.

The development would be located within an established residential area and would therefore not conflict with the local environment in terms of character and surrounding land uses. There is existing infrastructure in place to facilitate the single dwelling and the scale of the proposal would be such that there would not be over development of the site.

The principle is therefore considered to be acceptable and would comply with UDP Policies H1/2, H2/1, H2/2 and H2/6 and the principles of the NPPF.

Layout - The proposed dwelling would be located in the side garden of No 4 Mile Lane. It would be set 1m from the side elevation of No 4 and forward of the principle front elevation of this house by 2m. The building line is slightly staggered along this section of Mile Lane. The proposed dwelling would be positioned virtually in line with the frontage of No 4 and as

such considered it would have a comparable relationship within the context of the streetscene as the existing property.

There would be a distance of 6.8m from the side elevation of the proposed dwelling to the boundary with No 2 Mile Lane which would provide a generous separation distance and amenity area. At the rear, there would be a garden of 6.5m in depth to the new rear boundary of this property which is considered would provide an acceptable level of additional outdoor space. Beyond this, a strip of garden area some 5.5m deep would be retained in the ownership of No 4 to facilitate access to their summerhouse which is located in the south east corner of the existing garden.

The existing central access into the driveway of No 4 would become a shared access for both properties, with ample space to the front of each dwelling for parking.

The proposed siting of the dwelling is also constrained to account for the public sewer which crosses the site and which would require an access strip of 3m either side for maintenance or replacement purposes.

The proposed layout plan demonstrates that a dwelling and its associated requirements in terms of parking and amenity area could be successfully accommodated within the plot, reflecting the layout and arrangement of the existing properties in the area and in relation to the context of the streetscape.

As such, the layout is considered acceptable and would comply with H2/1 - The Form of Residential Development, H2/2 - The Layout of New Residential Development and H2/6 - Garden and Backland Development.

As a 'garden' development, and to protect the amenity of adjacent occupiers, it is considered to be appropriate and reasonable to remove permitted development rights.

Layout of No 4 Mile Lane - Whilst the development would result in a reduction to the garden area and driveway space for this property, there would still be ample and acceptable levels of parking provision and outdoor amenity space for this house without compromise to existing or future occupiers.

As such, the layout of No 4 Mile Lane would be considered acceptable and would comply with H2/1 - The Form of Residential Development, H2/2 - The Layout of New Residential Development and H2/6 - Garden and Backland Development.

Scale, design and appearance - The application proposes a 2 storey dwelling with a dormer in the rear roofspace. Whilst there is a bungalow adjacent at No 2 Mile Lane, No 4 is a large 2 storey house and there are houses opposite and in the surrounding area and as such it is considered that the proposed development would be acceptable in terms of housetype, scale and massing.

The height of the proposed dwelling would not be dissimilar to that of No 4 Mile lane and would follow the same eaves level, and with a pitch roof and gable ends would reflect the appearance of this property and others in the immediate vicinity. The proposed dwelling would also incorporate fenestration design patterns and the front elevation would feature a piked roof projection to add interest.

At the rear, the ground floor would project a further 1.6m and incorporate a monopitched roof with patio doors leading out to the garden area. A pitched roof dormer would be added at the rear, set in from the side elevations and rear wall of the dwelling. The size and scale of the dormer would result in a dominant addition or feature to the roof plane and would comply with the design guidance advised in SPD6.

Materials proposed are facing brickwork and concrete tiles. No 4 comprises brickwork elevations, whilst No 2 is entirely rendered. The houses in the area comprise of a mix of

materials and external finishes and as such the proposed materials are considered to be acceptable, subject to approval of details by condition.

As such, it is considered that the scale and design of the proposed dwelling would be appropriate within the context of the surrounding area and would comply with UDP Policies H2/1 - The Form of New Residential Development and EN1/2 - Townscape and Built Design and SPD6.

Impact on residential amenity - Whilst there are no set standards in terms of separation distances between these types of developments and residential properties, SPD6 is used as a guide to assess relationships and aspect standards between properties and new built development.

SPD6 states that a minimum distance of 20m should be maintained between habitable room windows, plus 3m for every additional storey or difference in levels, and 13m between habitable room windows and a 2 storey blank wall. Where there would be a difference in levels or additional storey, an additional 3m separation is generally sought.

No 14 Turton Close is a bungalow and would be directly face the rear elevation of the proposed dwelling. The ground level of this property is set approximately 1m lower and there is an intervening boundary fence. As the proposed development would have a dormer in the roofspace, a separation distance of 23m would be sought. There would be a distance of 24m from the dormer and 23m from the 2 storey element of the new build to No 14. There is also an intervening garden boundary fence which screens the ground floor of No 14 from the development site. The upper floors of the dwelling would be visible, but given the distance away and the intervening boundary treatment, the relationship between No 14 and the proposed dwelling would comply with policy guidance and is considered to be acceptable.

The rear elevation of No 12 Turton Close directly faces the rear elevation of the existing property, No 4 Mile Lane. There would be no direct interface between the new dwelling and No 12 Turton Close, and given the new dwelling would be off set from this property, and have a relationship which is commonly found between other properties, there would be no requirement nor are there any provisions within SPD6 guidance to apply separation distances to this situation.

There would be a distance of 6.8m from the proposed side elevation of the dwelling to the boundary with the bungalow at No 2 Mile Lane. There are 2 non habitable or secondary windows in the side elevation of this dwelling and there would be non habitable or secondary windows in the gable of the new dwelling facing this property but these would be obscure glazed. As such there would be no issues of overlooking to No 2 Mile Lane and the relationship to this property would be acceptable.

The proposed development would therefore comply with UDP Policies H2/1 - The Form of Residential Development, H2/2 - The Layout of New Residential Development, H2/6 - Garden and Backland Development and SPD6.

Access and parking - The existing southern access to No 4 Mile Lane would become shared access for both properties, with areas in front of the houses which could accommodate parking for at least 2 cars. The existing access to the north would be retained and used by No 4 Mile lane.

The access arrangements would not raise any highways issues or concerns, and parking could be comfortably provided within the site boundaries for both houses. The Highway's Section have raised no objections subject to conditions and as such the proposals would comply with H2/2 - The Layout of New Residential Development, H2/6 - Garden and Backland Development and HT2/4 - Car Parking and Servicing.

Coal Authority - The application site falls within a defined Development High Risk Area,

and therefore coal mining features and hazards which need to be considered in relation to the determination of the application.

The Coal Authority records indicate that the site is in an area of likely historic unrecorded underground coal mining workings at shallow depth.

A Coal Mining Risk Assessment has been submitted with the application and the Coal Authority have been consulted. The report sets out details of the intrusive site investigations and their findings. The Coal Authority concurs with the recommendations of the Coal Mining Intrusive Report that the coal mining legacy poses a risk to the proposed development and that remedial measures would be required to be undertaken prior to development.

A condition is therefore recommended requiring these remedial measures prior to commencement of development.

Response to objector

- Separation distances and aspect standards would be satisfied and the difference in levels has been accounted for in the assessment. There would be no requirement therefore to seek an alternative proposal.
- Permitted Development Rights would be removed by way of a condition to allow for assessment of future development of the site in terms of impact on residential amenity.
- The Bury Unitary Development Plan has remained the same in terms of what is assessed. However, the introduction of the NPPF represents a significant shift in Government Policy in how policies are interpreted. The general presumption of housing development within residential areas is a key supporting factor and in this instance the development has markedly changed from the 2004 application.
- For information, and in relation to proposals for a new dwelling in the garden of No 4 Mile lane only, the most recent application ref 60345 was withdrawn due to requirements to submit a Coal Report. This has been done and a condition would be included that a remediation strategy be submitted for approval.
- The application prior to this reference 43593 - Residential development - two storey detached three bedroom dwelling - was refused under delegated powers on 30/11/2004. In comparison to the current application, the refused scheme proposed a dwelling on a much larger scale and footprint which covered almost the entire width of the plot area. It was proposed 1.2m from the boundary with No 2, and projected further past the front and rear building lines of both Nos 2 and 4 Mile Lane. In terms of design, the front elevation proposed a 4.8m deep, 2 storey projection with a fully glazed elevation. As such it was considered by reason of its size, position and design, to be a prominent and intrusive feature in the street scene and would therefore, be seriously detrimental to the visual amenities of the area. As such it was refused.
- By contrast, this proposed scheme has been notably scaled down, and for the reasons in the above report, considered to be acceptable and would comply with policy guidance.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered P362/01A/02A/03B/04/05E/06 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
5. Following the provisions of Condition 3 of this planning permission, where ground gas remediation / protection measures are required, the approved Remediation Strategy must be carried out to the written satisfaction of the Local Planning Authority within approved timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.
Reason. To alleviate any possible risk associated with the production of landfill gas and ground gas in accordance with the recommendations of the Environment Agency and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
6. No development shall commence unless and until a scheme of remedial works has been submitted to and approved by the Local Planning Authority. The approved remedial works only shall be implemented and in accordance with the approved scheme.
Reason. Information has not been submitted at application stage, which is required to ensure the safety and stability of the proposed development and to alleviate possible risk pursuant to chapter 11 - Conserving and enhancing the

natural environment.

7. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the Local Planning Authority and shall confirm/provide the following:

- Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
- Parking on site of operatives' and construction vehicles together with storage on site of construction materials.
- Measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials.

Reason. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent classified road and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety and to ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Bury Unitary Development Plan Policy EN1/2 - Townscape and Built Design.

8. The turning facilities indicated on approved plan reference P362/05 Revision E shall be provided prior to the dwelling hereby approved being occupied and the areas used for the manoeuvring of vehicles shall subsequently be maintained free of obstruction at all times.

Reason. To minimise the standing and turning movements of vehicles on the highway in the interests of road safety pursuant to Bury Unitary Development Plan Policies H2/2 - The Layout of New Residential Development, H2/6 - Garden and Backland Development and HT6/2 - Pedestrian and Vehicular Conflict.

9. The car parking indicated on approved plan reference P362/05 Revision E shall be surfaced, demarcated and made available for use to the satisfaction of the Local Planning Authority prior to the dwelling hereby approved being occupied and thereafter maintained available for use at all times.

Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policies H2/2 - The Layout of New Residential Development and HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.

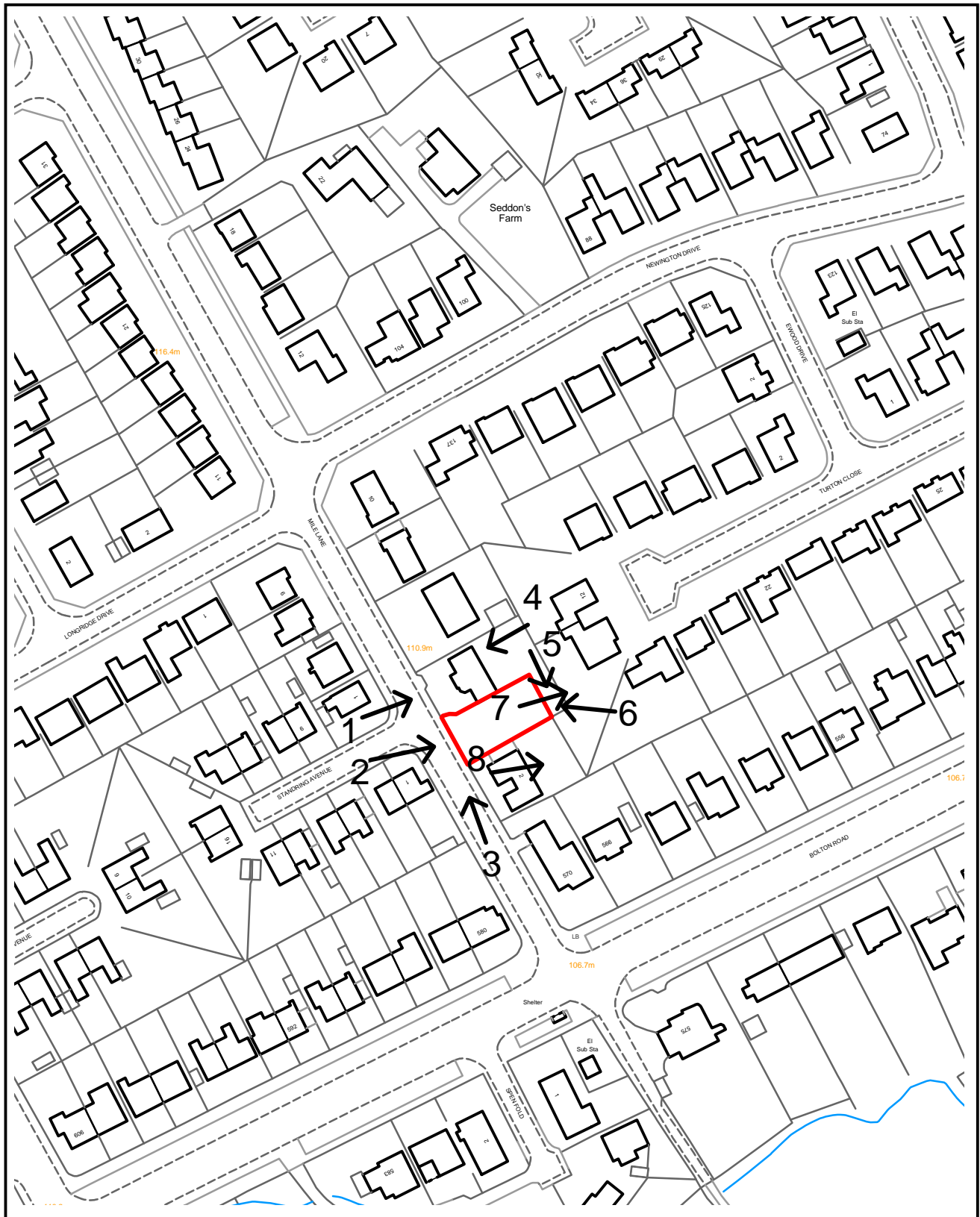
10. No development shall commence unless and until details of surface water drainage proposals have been submitted to and approved by the Local Planning Authority. The scheme should be in accordance with the submitted Surface Water Sustainable Drainage Assessment and must be based on the hierarchy of drainage options in the National Planning Practice Guidance and be designed in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015). This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Details of proposed maintenance arrangements should also be provided. The approved scheme only shall be implemented and thereafter maintained.

Reason. To promote sustainable development. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact pursuant to chapter 10 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.

11. Notwithstanding the terms of the Town and Country Planning (General Permitted Development) (England) Order 2015, as subsequently amended, no development shall be carried out within the terms of Classes A to G of Part 1 of Schedule 2 of the Order, without the submission and approval of a relevant planning application. Reason. To ensure that future inappropriate alterations or extensions do not occur pursuant to policies of the Unitary Development Plan listed.
12. Before the first occupation of the dwelling hereby approved the windows in the south (side) elevation shall be fitted with obscured glazing and shall be permanently retained in that condition thereafter. Reason. To protect the privacy of adjoining occupiers and to accord with Policy H2/1 - The Form of New Residential Development and H2/6 - Garden and Backland Development of the Bury Unitary Development Plan and Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 61286

ADDRESS: Land at 4 Mile Lane
Bury

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

61286

Photo 1



Photo 2



Photo 3



Photo 4



Photo 5



Photo 6

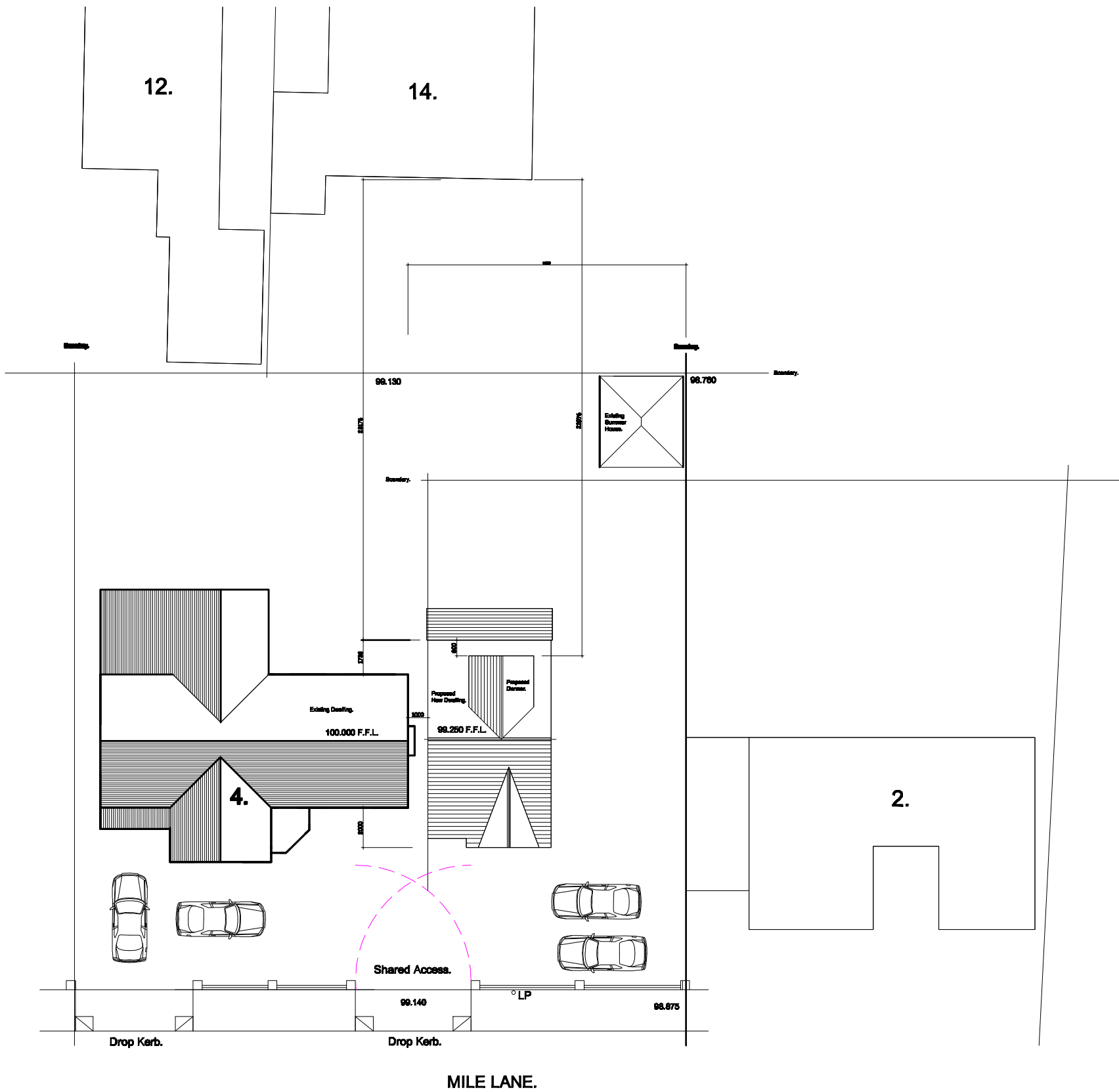


Photo 7



Photo 8





PROPOSED SITE PLAN.

Revision	A - Site Levels Added. - 22nd July 2016. B - Amended Footprint. - 6th September 2016. C - Drop Kerb Added. - 19th September 2016. D - Access from Mile Lane Amended. - 22nd September 2016. E - Access from Mile Lane Amended. - 27th September 2016.
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Title :
PROPOSED NEW DWELLING.

Project :
LAND ADJACENT TO 4, MILE LANE,
BURY.
BL8 2DS

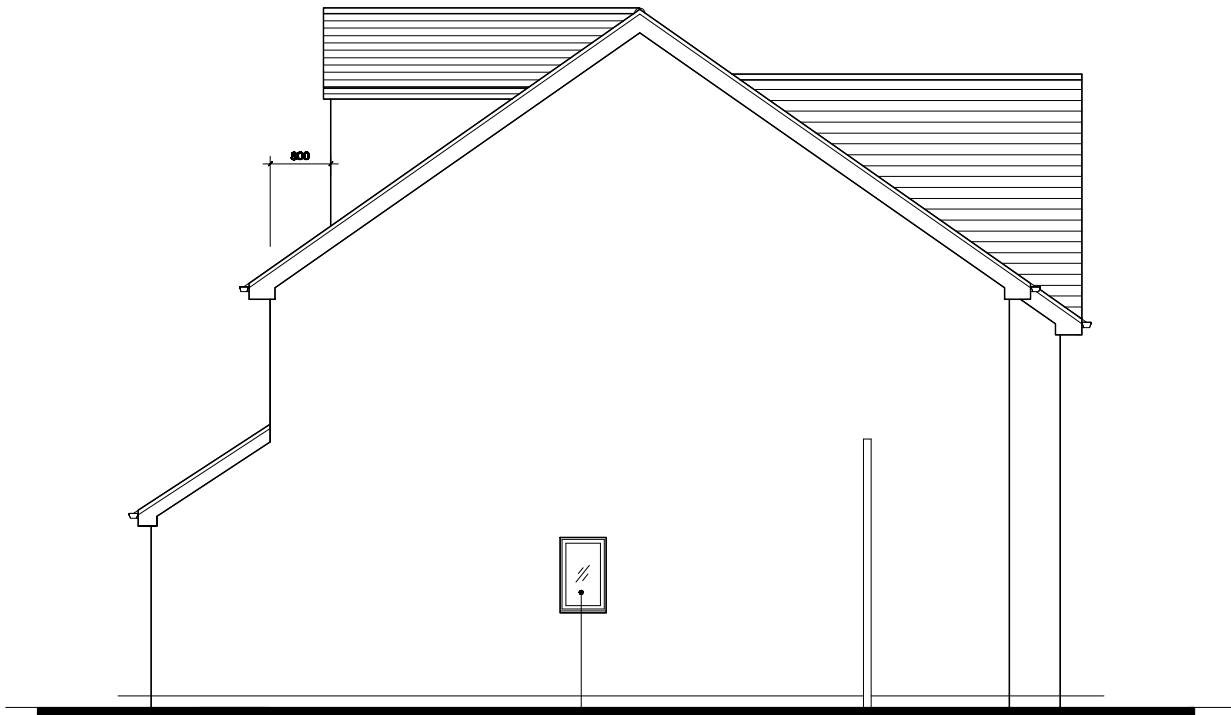
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Scale: 1:250
Date: May 2016.

Drawing No: P362/05E



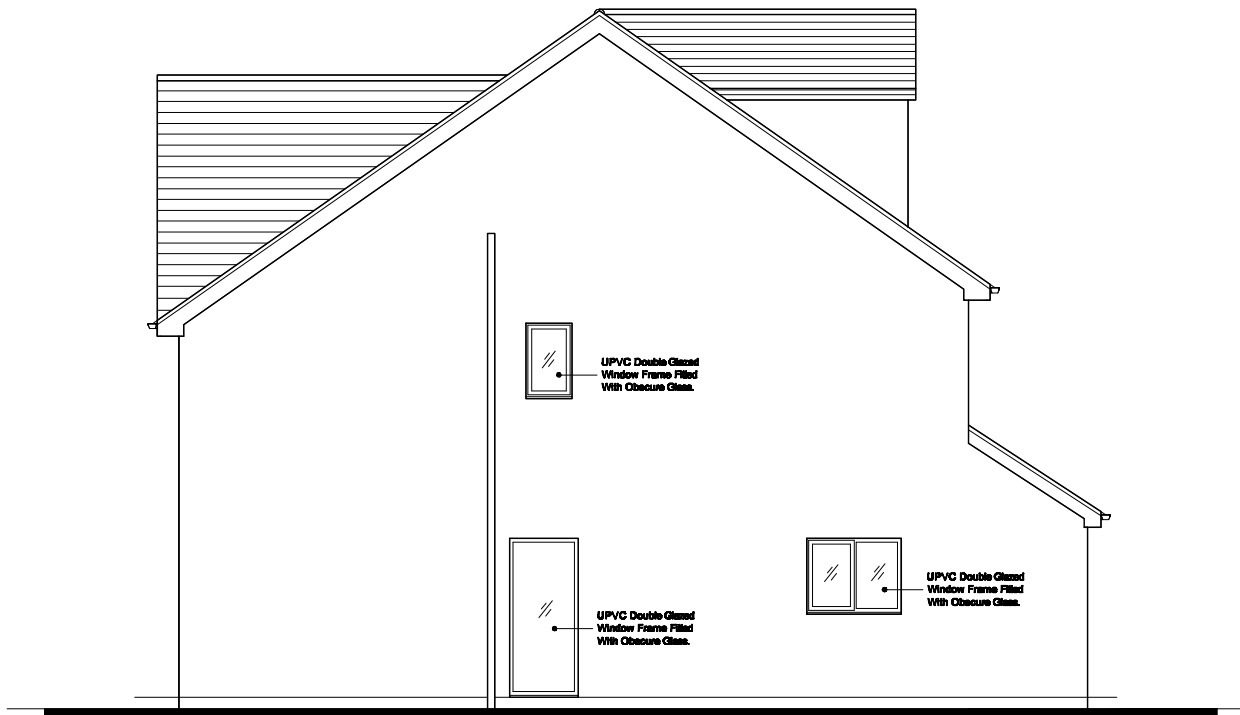
PROPOSED FRONT ELEVATION.



PROPOSED SIDE ELEVATION.



PROPOSED REAR ELEVATION.



PROPOSED SIDE ELEVATION.

Revision A - Dimension Added For Dormer. - 6th September 2016.
B - Ground Floor Windows Amended. - 2nd November 2016.

Title :
PROPOSED NEW DWELLING.

Project :
LAND ADJACENT TO 4, MILE
LANE,
BURY.
BL8 2DS

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Scale 1:100
Date: May 2016

Drawing No: P362/03E



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EXISTING FRONT ELEVATION.



PROPOSED FRONT ELEVATION.

Revision

Title :
PROPOSED NEW DWELLING.

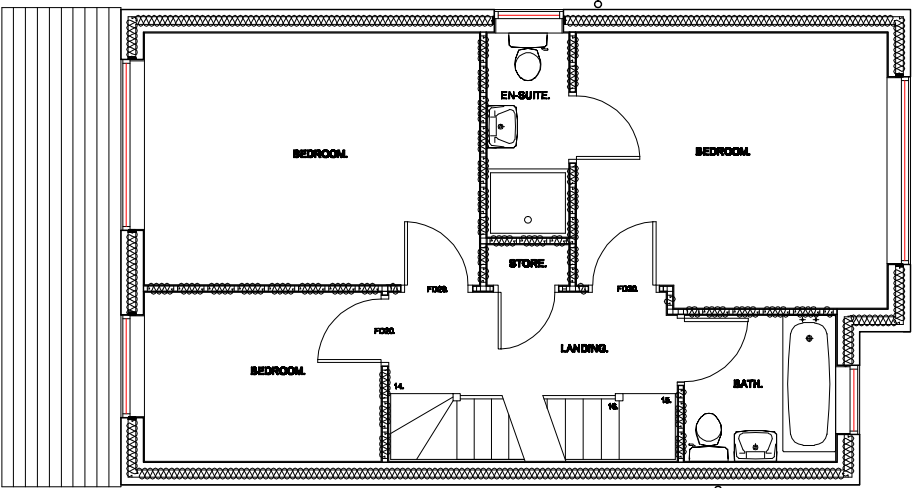
Project :
LAND ADJACENT TO 4, MILE
LANE,
BURY.
BL8 2DS

MTC Designs

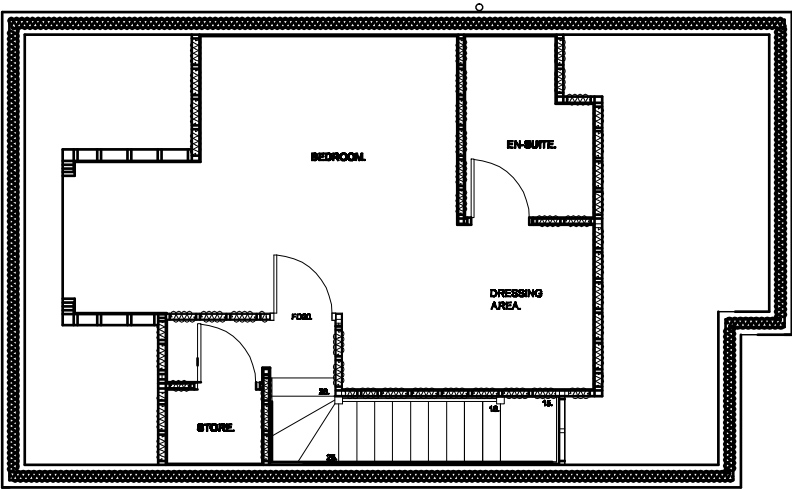
31 Exford Drive Bolton BL2 6TB
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Scale 1:100
Date: May 2016

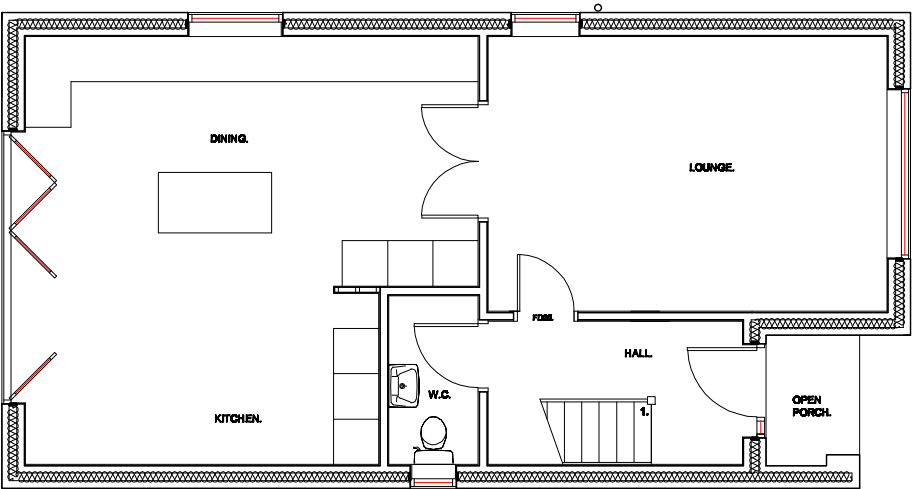
Drawing No: P362/06



PROPOSED FIRST FLOOR PLAN.



PROPOSED SECOND FLOOR PLAN.



PROPOSED GROUND FLOOR PLAN.

Revision

Title :
PROPOSED NEW DWELLING.

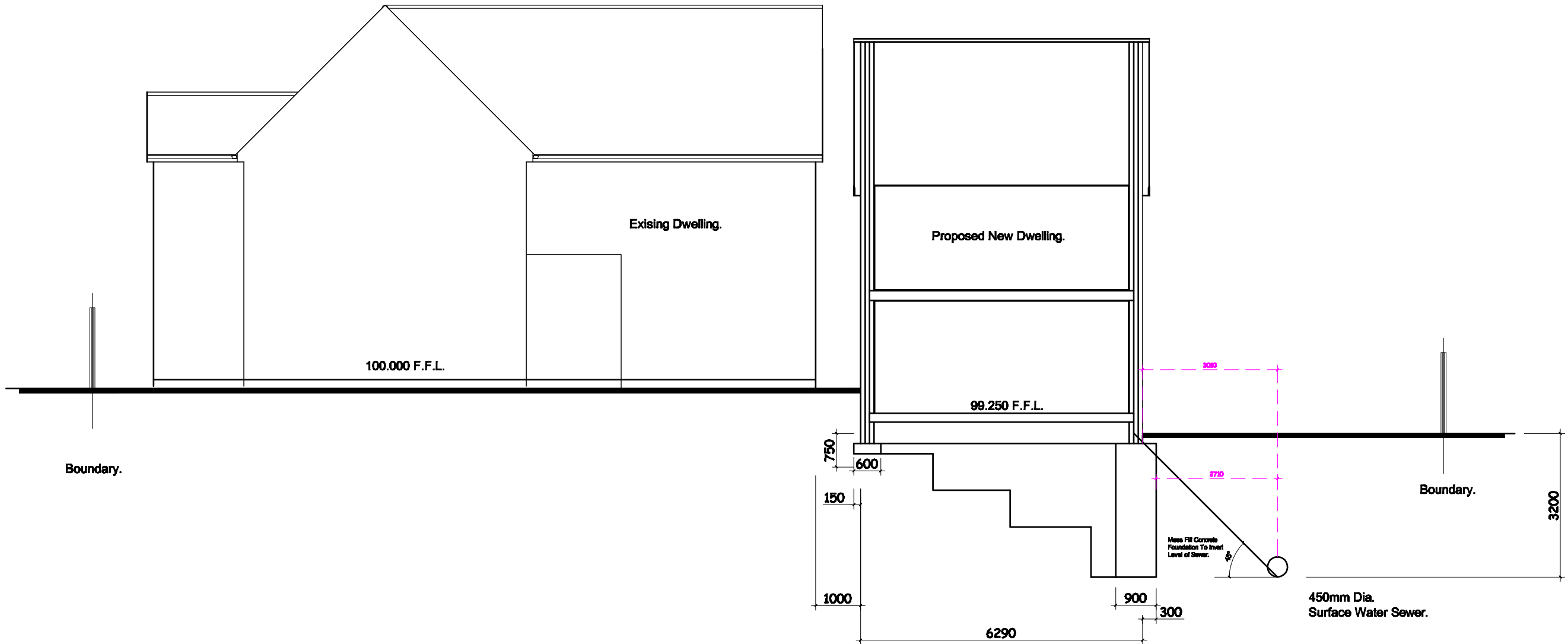
Project :
LAND ADJACENT TO 4, MILE
LANE,
BURY.
BL8 2DS

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Scale 1:100
Date: May 2016

Drawing No: P362/04



SECTION.

Revision A - Site Levels Added. - 22nd July 2016.

Title :
PROPOSED NEW DWELLING.

Project :
LAND ADJACENT TO 4, MILE
LANE,
BURY.
BL8 2DS

MTC Designs

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Scale 1:100
Date: May 2016

Drawing No: P362/02

Ward: Prestwich - Sedgley

Item 06

Applicant: Mr Tony Freeman

Location: 130 Bury New Road, Prestwich, Manchester, M25 0AA

Proposal: Two storey side extension to provide additional retail space (Class A1) at ground floor level and office space (Class B1a) at first floor level with associated car park alterations and landscaping works

Application Ref: 61296/Full

Target Date: 19/05/2017

Recommendation: Approve with Conditions

Description

The application relates to an existing kitchen showroom on the west side of the L-shaped block of commercial buildings on the corner of Bury New Road and Scholes Lane. Although the site lies just outside the designated Neighbourhood Shopping Centre, it is, for all intents and purposes, an integral part of the centre.

There is a car park with 9 spaces serving the shopping centre immediately to the north west. Beyond the car part is a landscaped communal garden, known as Holden Gardens and beyond this houses fronting Hope Park Close. Immediately to the north is a Chinese restaurant with its own car park to the front.

The proposal is to extend out from the north east elevation, facing onto the car park. The extension would be set back towards the rear of the building and would have a rectangular footprint measuring 8.4m by 5.5m. With elevations comprising a high degree of glazing with metal profile panels and a flat roof, the idea is to have a more contemporary appearance that reflects to an extent the existing building.

The ground floor would accommodate additional 40sqm of retail space whilst above there would be 40sqm of office space. To accommodate the extension the existing car park would be extended into part of the adjacent landscaped garden and reconfigured to maintain 9 parking spaces. As part of the scheme a tree survey by Bowland Tree Consultancy was undertaken and submitted with the application. The tree survey identified one Cherry tree as poor in quality and not suitable for retention. The report states that in mitigation for the loss of the Cherry tree, four new trees would be planted along the boundary with the car park.

Relevant Planning History

01629/E - Proposed erection of three storey side extension to create additional A3 class use space at ground floor level and additional office space at first and second floor levels - Enquiry completed 26/03/2015

47931 - Change of use of ancillary store to pizza take-away (A5) to ancillary office for showroom (A1) and alterations to rear and side elevations - approved with conditions 05/06/2007

Publicity

A site notice was posted and the following 33 properties were notified by letter dated 29/03/17. Nos.1-7 (odd) Scholes Lane, 132, 153 Bury New Road, 27 and 33 Hope Park Close, Butt Hill Court, 1-7 (odd) Rowan Place

Two objections have been received by Mr Cabrelli and Mr Isaacson who are joint owners of the access road and adjacent property.

- The new building will take up and reduce the existing parking spaces for staff and

- visitors to Maxdov House 130 Bury New Rd including the shops and Pizza Express,
- The new building will bring in yet more staff and visitors with not only no extra parking places but in fact several less than as at present,
- The access road (which is jointly owned by myself and Mr Isaacson) is already obstructed by cars parked along both sides of it .The view round the bend in the access road is obstructed by those parked vehicles and it is frequently difficult for a vehicle to drive along it if another vehicle is coming in the opposite direction.,
- Parking in front of Saporito Restaurant and The Coffee Sack coffee shop at 3 Scholes Lane, (also now jointly owned by myself and Mr Isaacson), is restricted to customers of those two establishments, and would not be available for the proposed new building extension.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection.

Drainage Section - No objection.

Environmental Health - No objection.

Greater Manchester Police - No objection.

United Utilities - No objection.

Unitary Development Plan and Policies

S1/5	Neighbourhood Centres and Local Shops
EN1/2	Townscape and Built Design
HT5/1	Access For Those with Special Needs
EN1/2	Townscape and Built Design
EC4/1	Small Businesses
EC5/3	Other Office Locations

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policy - S1/5 - Neighbourhood Centres and Local Shops. The Council will seek to retain retailing (Class A1), as the predominant use in small neighbourhood centres and in new or existing local shops, to cater primarily for the day to day needs of residents and businesses.

S2/1 - All New Retail Proposals: Assessment Criteria. The Council will support new retail development proposals which accord satisfactorily with the following factors:

- a) are within or immediately adjoining the main shopping area of existing centres;
- b) sustain or enhance the vitality and viability of a centre;
- c) are accessible by and would encourage greater use of public transport;
- d) are in conformity with other policies and proposals of the Plan.

Having successfully established the principle of development, all retail proposals will be considered with regard to their environmental impact and must take account of the following factors:-

- e) they should have regard to their surroundings in terms of design, scale, height, bulk, use and colour of materials and landscaping;
- f) the effects on the environment of the area as a result of increased traffic generation and servicing;
- g) the design of the proposal with regard to safety and security for shoppers, workers, visitors and surrounding occupiers;

- h) the effects on the amenity of nearby residents or businesses by reason of noise, smell, litter or opening hours;
- i) access and, where appropriate through negotiation, facilities for the mobility impaired;
- j) the provision of associated facilities. Where applicable, proposals should attempt to provide an appropriate level of additional community benefit and/or offset the loss of or impact on any amenity or resource present on the site. The following facilities serve as examples; creche/children's play, baby changing and feeding, public conveniences (including provision for the disabled) and recycling facilities;
- k) the provision of adequate car parking and servicing.

EN1/2 - Townscape and Built Design. The Council will give favourable consideration to proposals which do not have an unacceptable adverse effect on the particular character and townscape of the Borough's towns, villages and other settlements. Factors to be considered when assessing proposals will include:

- a) the external appearance and design of the proposal in relation to its height, scale, density and layout;
- b) the relationship of the proposal to the surrounding area;
- c) the choice and use of materials;
- d) access and other design features for the mobility impaired;
- e) the design and appearance of access, parking and service provision;
- f) landscaping, including the use of natural landscape features, and open space provision;
- g) the use of lighting.

Visual amenity and Streetscape - The extension appears well designed and proportionate to the existing building. Whilst the colour scheme mirrors that of the main building, the materials and general design elements are more contemporary. With the new build being set back well into the site and appearing subservient to the main building, the proposal is considered to be in keeping with the existing building and generally appropriate within the wider streetscape.

The alterations to the car park, including the extension onto part of the landscaped garden at the side are relatively modest and would not have a significant impact on the overall appearance of the area. The loss of a narrow wedge of landscaped area at the edge of the car park would be in part mitigated by the additional tree planting along the boundary. Overall, the impact on the communal garden area would not be significant and all but one of the existing trees, a cherry tree, would be retained. A suitable condition would ensure that the four trees, to be planted in mitigation, are planted and maintained thereafter.

As such the proposal is acceptable in design terms and complies with UDP Policies EN1/2 Townscape and Built Design and S2/1 All New Retail Proposals.

Residential amenity - The nearest residential properties are houses to the north, on Hope Park Close. The house closest to the boundary with the site, No.31 has a blank gable facing the car park. The gable would be approximately 20m away from the extension. Given that the extension would be relatively modest in scale and set well away from the nearest residential properties, in terms of residential amenity, there are no significant issues arising.

Access and Parking - The existing car park and access road are well used during the day. The proposal does not involve the creation of addition parking spaces, but a reconfiguration of the nine existing spaces.

The Council's existing guidance on parking, in SPD11 Parking Standards in Bury, suggests that additional retail and office space created would require approximately two additional parking spaces, although the figures are based on maximum standards.

The applicant argues in the Design and Access Statement that there has been no reported issues of overflow parking in surrounding residential streets, the site is in a high access area with good transport links and that the extension is relatively modest and would not result in undue additional pressure on parking. It is also stated that there would be additional cycle parking provided on site.

Given that the additional parking requirements as set out in the Council's guidance are marginal, it is not considered that there is a strong reason to refuse this application on parking grounds and to maintain the provision at current levels is sufficient and policy compliant.

Servicing - The servicing arrangements and bin storage at the rear would remain as existing and unaffected.

Objection - The parking issues raised by the objectors have been addressed in the above report. The concerns raised about cars parking along the private access road, jointly owned by the objectors, is a private matter.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 4772-01-01, 02A, 03, 04A, 05, 06A, 07A, 08, 09 and 10 and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details of the materials to be used in the external elevations, together with details of their manufacturer, type/colour and size, shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/2 Townscape and Built Design.
4. The car parking indicated on the approved plans shall be constructed, demarcated and made available for use prior to the extension hereby approved being brought into use and thereafter maintained.
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to policy HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.
5. All trees to be retained on site shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are

implemented and all measures required shall remain in situ until the development has been completed.

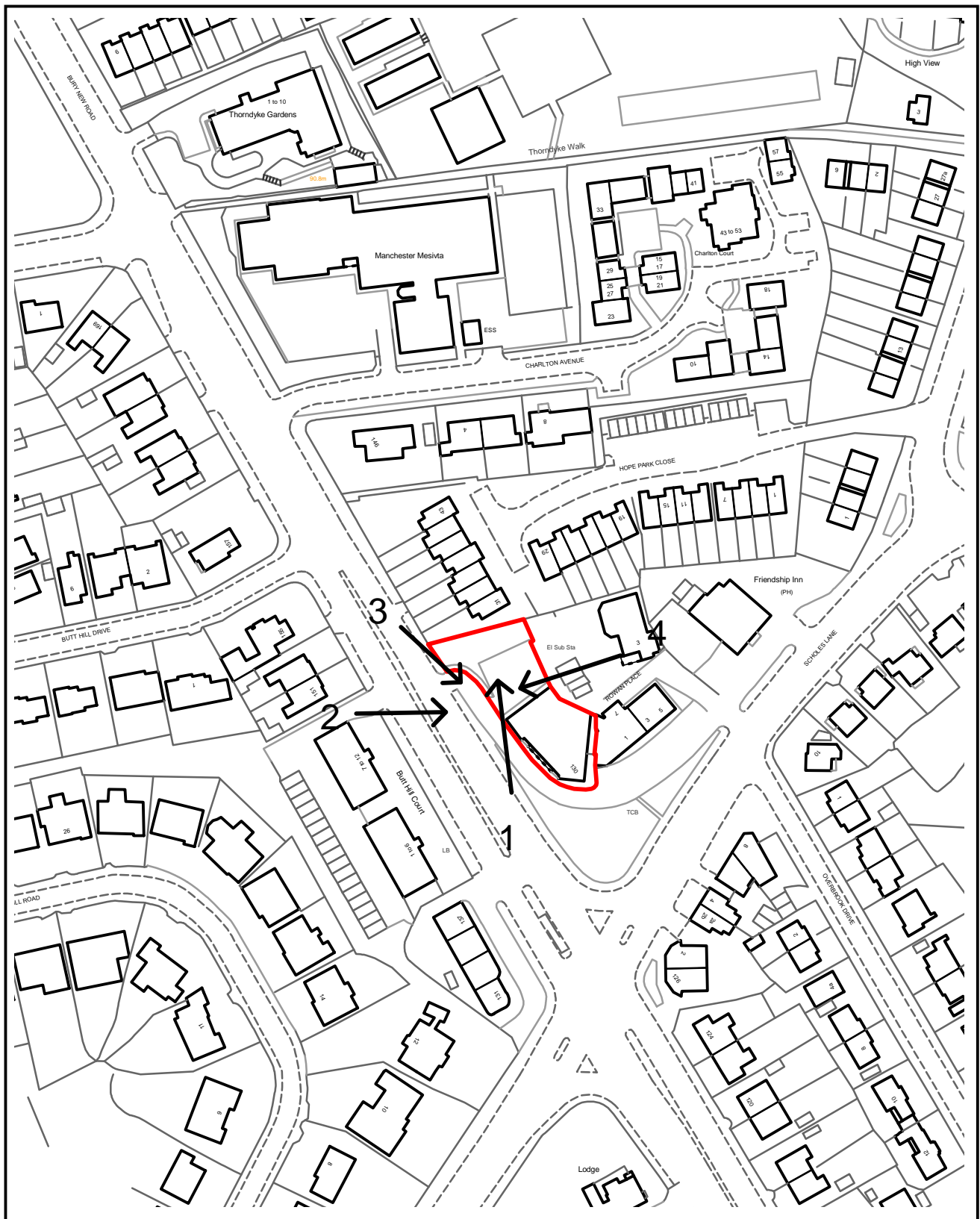
Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

6. A landscaping scheme, including the new tree planting proposals, shall be submitted to, and approved in writing by, the Local Planning Authority prior to the commencement of the development. It shall be implemented not later than 12 months from the date the car park is brought back into use; and any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted to the written satisfaction of the Local Planning Authority.

Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



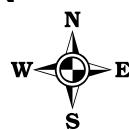
PLANNING APPLICATION LOCATION PLAN

APP. NO 61296

**ADDRESS: 130 Bury New Road
Prestwich**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

61296

Photo 1



Photo 2

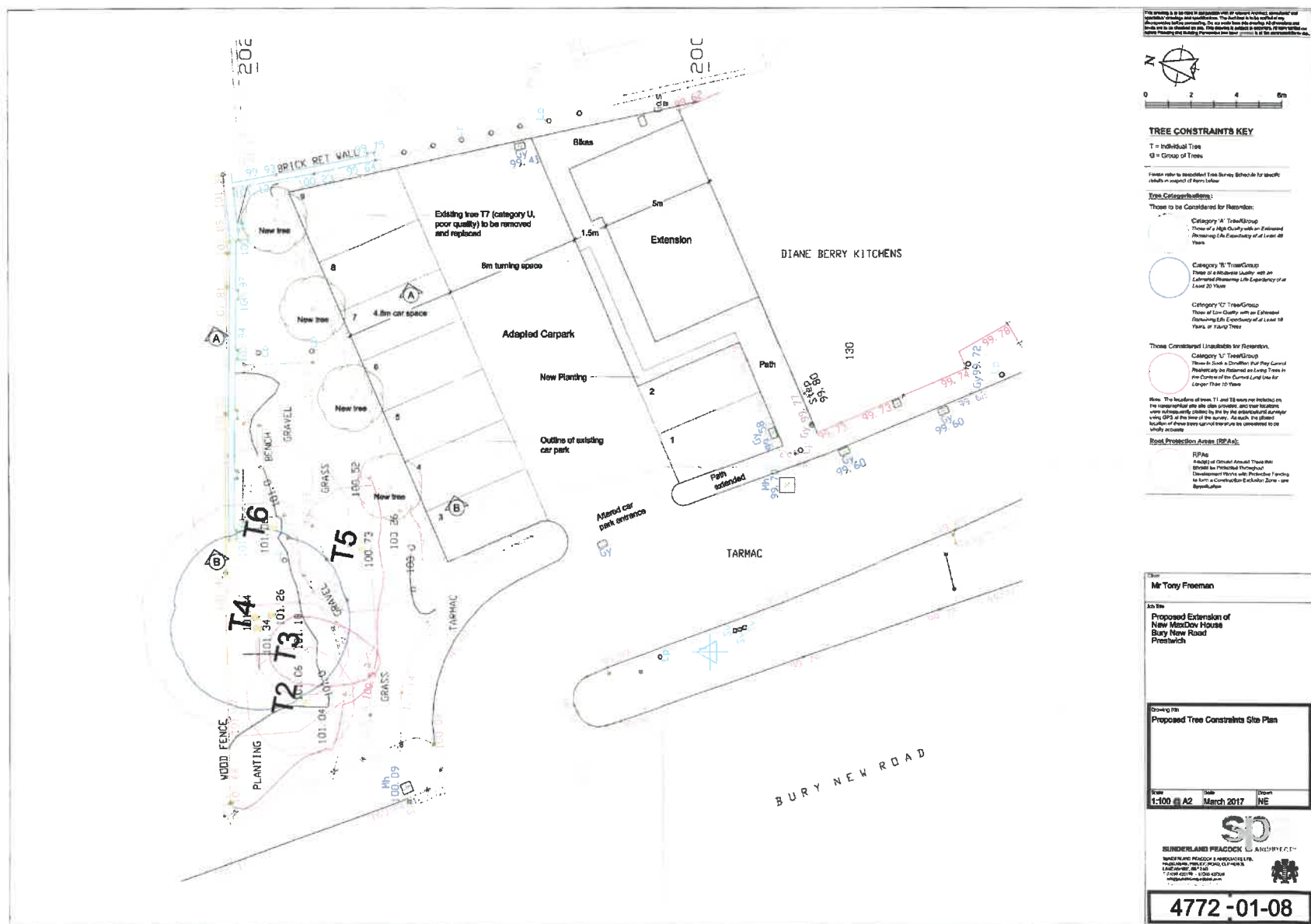


Photo 3

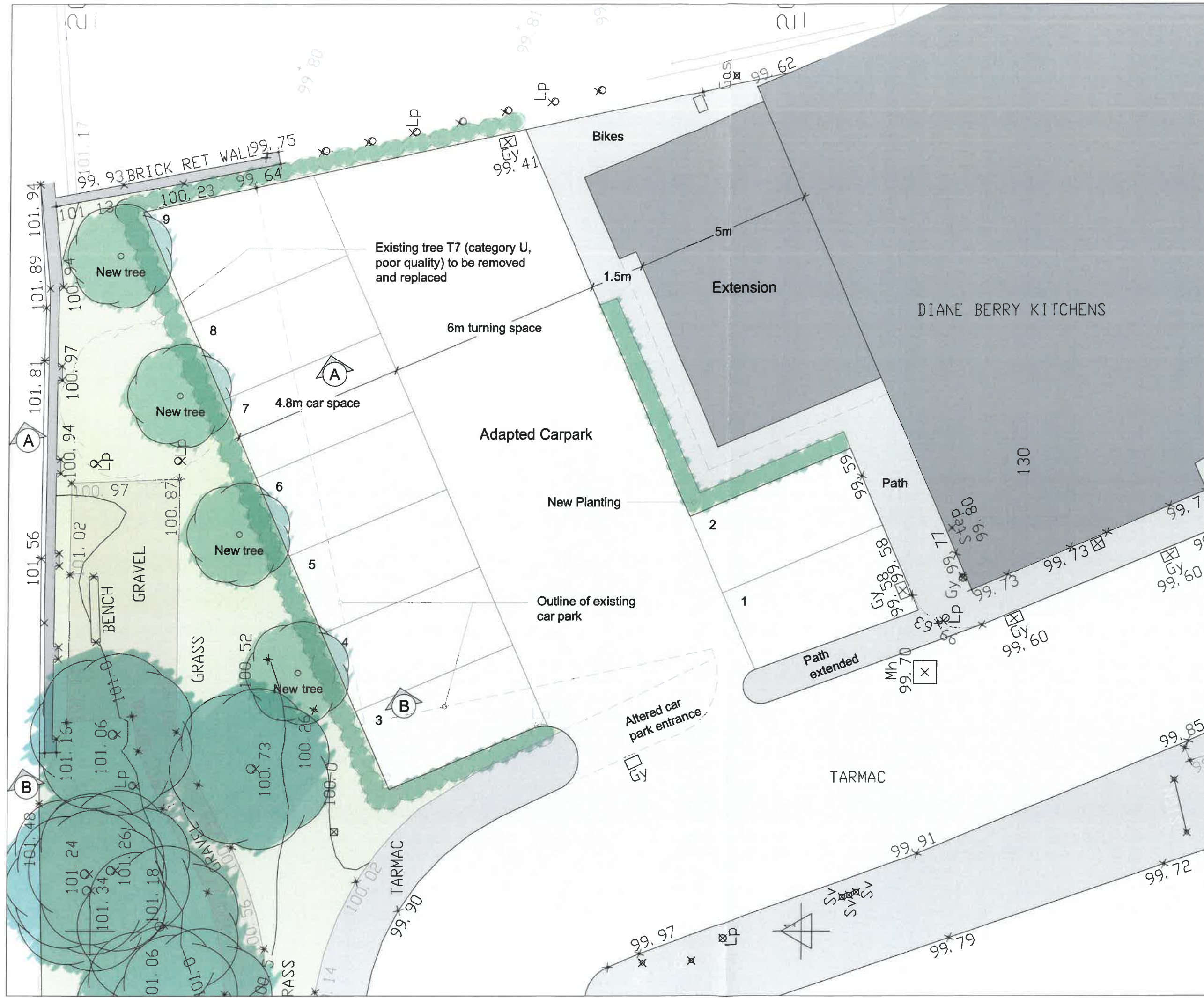
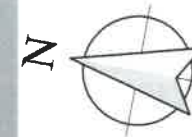


Photo 4





This drawing is to be read in conjunction with all relevant Architect, consultants' and specialists' drawings and specifications. The Architect is to be notified of any discrepancies before proceeding. Do not scale from this drawing. All dimensions and levels are to be checked on site. This drawing is subject to copyright. All work carried out before Planning and Building Permission has been granted is at the contractor/client risk.



REVISIONS:
Rev A - Scheme amended. - 02.03.17 - NE

Client
Mr Tony Freeman

Job Title
Proposed Extension of
New MaxDov House
Bury New Road
Prestwich

Drawing Title
Proposed Site Plan

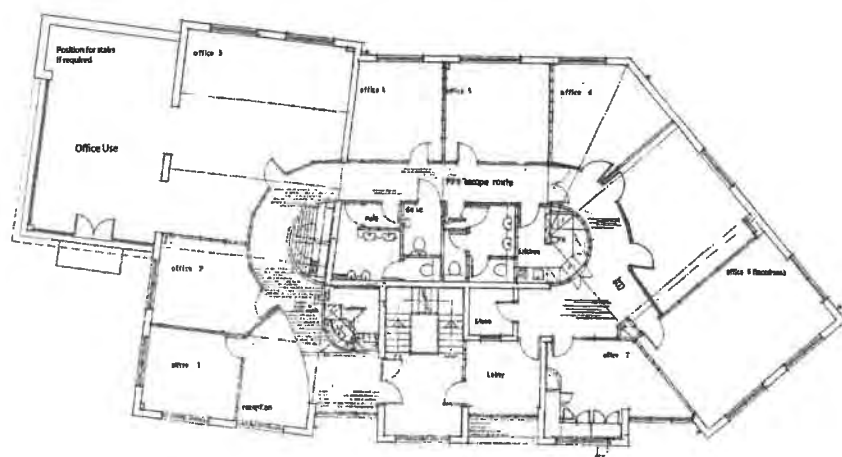
Scale 1:100 @ A3	Date Feb 2017	Drawn NE
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spa ARCHITECTS
SUNDERLAND PEACOCK & ASSOCIATES LTD.
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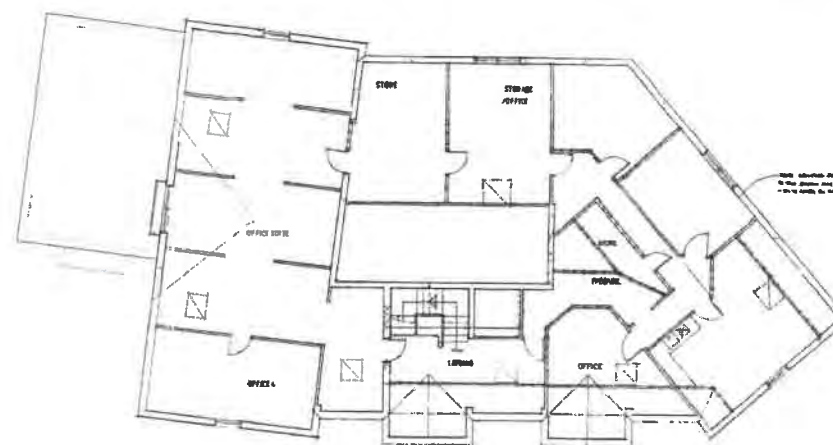


4772-01-07 Rev A

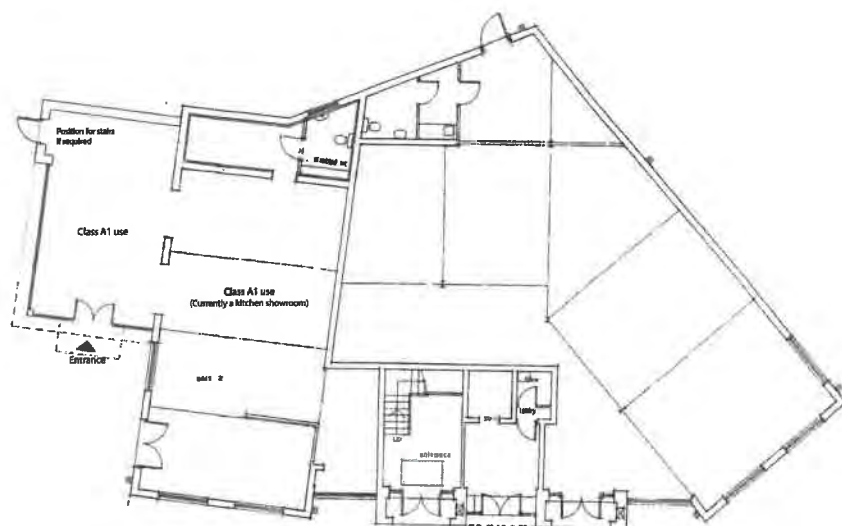
Note: Internal layouts are provisional and subject to Structural Engineers input



First Floor Plan



Second Floor Plan



Ground Floor Plan

Mr Tony Freeman

**Proposed Extension to
New MaxDor House
Bury New Road
Prestwich**

Drawing Title
Proposed Plans

1:100 A1

spa
ARCHITECTS

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4772 - 01-05



Existing North-West Elevation



Existing South-West Elevation



Proposed North-West Elevation



Proposed South-West Elevation

Mr Tony Freeman

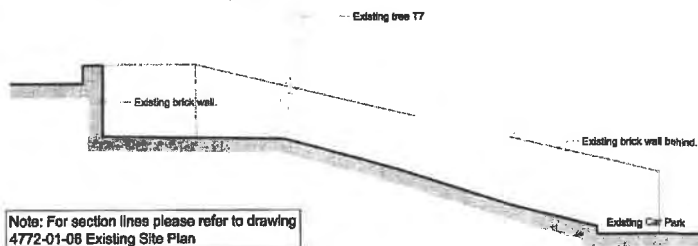
Proposed Extension of
New Max Day House
Sury New Road
Prestwich

Showing the
Existing and Proposed Elevations

Scale: 1:100
Date: March 2017
Drawn: NE

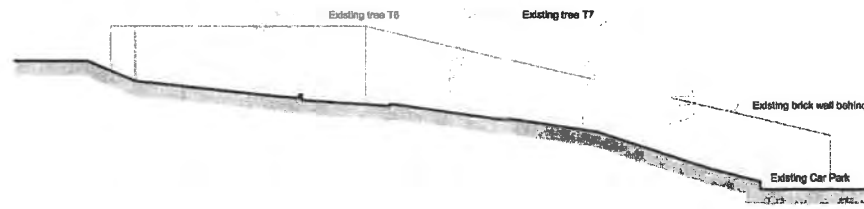
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ARCHITECTS
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4772-01-10

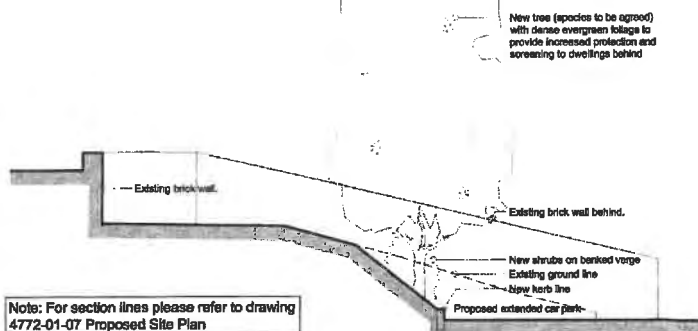


Datum +99.00m

Existing Section A-A

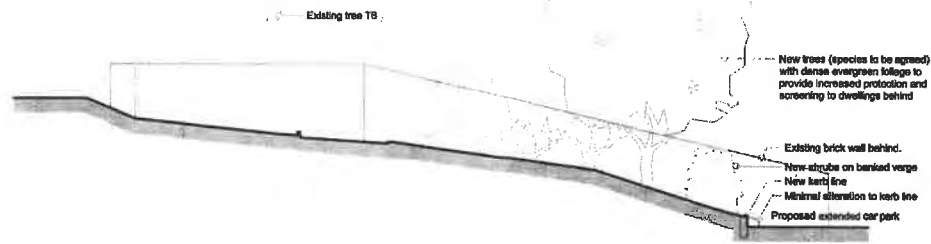


Existing Section B-B



Datum +99.00m

Proposed Section A-A



Proposed Section B-B

Mr Tony Freeman

Job title

Proposed Extension of
New MaxDov House
Bury New Road
Prestwich

Drawing title

Existing and Proposed Site Sections

Scale
1:50 @ A2

Date
March 2017

Drawn
NE

spa

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4772-01-09



Revisions
Rev A - Landscaping scheme amended.
04.03.17 NE

Client
Mr Tony Freeman

Job Title
**Proposed Extension to
MaxDov House
Bury New Road
Prestwich**

Drawing Title
Proposed view from Bury New Road

Scale NTS @ A3	Date Feb 2017	Drawn NE
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4772 - 01-02 REV
A

Ward: Ramsbottom and Tottington -
Ramsbottom

Item 07

Applicant: Mr Robert Hodkinson

Location: Site of former police station, Bridge Street, Ramsbottom, Bury, BL0 9AB

Proposal: Creation of car park for temporary period of 3 years

Application Ref: 61299/Full

Target Date: 19/05/2017

Recommendation: Approve with Conditions

This application is being presented to Planning Control Committee as the applicant is a Councillor.

Description

The application site formerly contained 2 buildings, comprising a former police station and garaging. These buildings have been demolished recently. Vehicular and pedestrian access is taken from Bridge Street with a second pedestrian access onto Garden Street. There is a brick wall with timber fencing above on the boundary to Garden Street.

The site is located within the Ramsbottom Conservation Area and is a town centre site. St Paul's church is located to the north west of the site and is a Grade II listed building with memorial gardens to the west. The Natwest Bank is located to the east and there is a post box adjacent to the gardens, which are both on the draft local list. There are apartments to the north and retail units with residential accommodation above to the south.

Planning consent was granted in January 2017 for the demolition of the police station and garage buildings and the erection of a three storey building. The proposed building would contain 2 retail units on the ground floor and 8 apartments above. The proposed building would be located on the Bridge Street frontage and would be constructed from stone with a slate roof. Pedestrian access would be achieved from Bridge Street. No parking is proposed and a yard would be located to the rear of the building. Servicing would take place on Bridge Street.

The proposed development involves the creation of a car park for a temporary period of 3 years. The proposed car park would be used by business owners, who currently have difficulty in finding long term, daily parking in the town centre. The proposed spaces would be demarcated using suitable wooden/metal pins driven into the ground and a 600mm timber knee rail would be located along the boundary with Bridge Street. The existing boundary treatments would be retained.

The site would be accessed from the central vehicular access on Bridge Street and would have restricted access to contract permit holders. The car park would be unmanned during normal weekdays and weekends when the town's businesses are open for trading.

Relevant Planning History

01848/E - Proposed demolition of former police station and erection of 2 no. shop units with 8 no. apartments over - Enquiry completed 09/08/2016

60708 - Demolition of former police station and erection of building to include 2 no. shop units with 8 no. apartments above at former Police Station, Bridge Street, Ramsbottom. Approved with conditions - 25 January 2017.

Enforcement

17/0069 - Breach of conditions at former Ramsbottom Police Station, Bridge Street, Ramsbottom. Breach established - 13 February 2017 - Application to discharge conditions is awaited.

Publicity

The neighbouring properties were notified by means of a letter on 31 March 2017 and a press notice was published in the Bury Times on 6 April 2017. Site notices were posted on 4 April 2017.

2 letters have been received from the occupiers of 7, 11 Garden Street, which have raised the following issues:

- The air pollution in Ramsbottom is terrible and the proposed development would bring cars within 20 feet of my living room.
- Some of my neighbours suffer from asthma and chronic obstructive pulmonary disease (COPD).
- The nitrogen dioxide limit will be over the legal limit.
- During the chocolate festival a bouncy castle was put on this land and a children's climbing frame. The diesel generator was clattering away with no noise reduction cladding from 7am and children were screaming all day from 9am - 4pm.

The objectors have been notified of the Planning Control Committee meeting.

Consultations

Traffic Section - No objections in principle. Further comments to be reported in the Supplementary Report.

Drainage Section - Comments awaited.

Design for security - Comments awaited.

Unitary Development Plan and Policies

EN1/1	Visual Amenity
EN1/2	Townscape and Built Design
EN1/3	Landscaping Provision
EN1/5	Crime Prevention
EN2/1	Character of Conservation Areas
EN2/2	Conservation Area Control
EN2/3	Listed Buildings
EN7/5	Waste Water Management
S1/2	Shopping in Other Town Centres
HT2/4	Car Parking and New Development
HT2/5	Public Car Parks
Area	St Paul's/Crow Lane
RM2	
NPPF	National Planning Policy Framework

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Impact upon conservation area - The national legislative framework for development affecting listed buildings and conservation areas is provided by the Planning (Listed Buildings and Conservation Areas) Act 1990. This sets out the duty on local planning authorities with regard to listed buildings and any buildings or land within a conservation

area, when determining applications for planning permission. It is essential that these legal duties are considered, alongside the contents of the NPPF and other planning policies and guidance.

The Town and Country Planning (Listed Building and Conservation Area) Act 1990 imposes a duty under Section 72 on Local Planning Authorities to pay 'special attention to the desirability of preserving or enhancing the character or appearance of that area' (section 72(1))

Paragraph 131 of the NPPF states that when determining planning applications, Local Planning Authorities should take account of:

- the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- the positive contribution that conservation of heritage assets can make sustainable communities including their economic vitality;
- the desirability of new development making a positive contribution to local character and distinctiveness.

Paragraph 133 of the NPPF states that where a proposed development would lead to substantial harm or total loss of significance of a designated heritage asset, local planning authorities should refuse consent, unless it can be demonstrated that the substantial harm or loss is necessary to achieve substantial public benefits that outweigh that harm or loss, or all of the following apply:

- the nature of the heritage asset prevents all reasonable uses of the site; and
- no viable use of the heritage asset itself can be found in the medium term through appropriate marketing that will enable its conservation; and
- conservation by grant funding or some form of charitable or public ownership is demonstrably not possible; and
- the harm or loss is outweighed by the benefit of bringing the site back into use.

Policy 24 of the Conservation Area management plan identifies the Police station buildings as an "issue to be addressed due to their design and appearance."

The existing police station buildings have been demolished and the existing surface of tarmac and concrete would be retained. The proposed car parking spaces would be demarcated with wooden/metal pins driven into the ground, which would be appropriate within the conservation area and near a listed building. A timber knee rail would be provided along the boundary to Bridge Street, which would be acceptable and the existing boundary treatments would be retained.

The proposed development would be used by contract permit holders, who would gain access during normal weekdays and weekends and this would be controlled by two bollards. The proposed bollards would be timber and would be appropriate to the conservation area. Therefore, the proposed development would preserve the character of the conservation area and the adjacent listed building and would be in accordance with Policies EN2/1, EN2/2 and EN2/3 of the Bury Unitary Development Plan.

Highways issues - The proposed development would re-locate the access to a central position, which would ensure good inter-visibility between pedestrians and vehicles. The proposed access would be widened to ensure that two cars can enter and exit the car park at the same time to ensure the free flow of traffic in the town centre. Revised plans have been provided that ensures there is adequate manoeuvring space in the car park. The Traffic Section has no objections in principle and further comments will be reported in the Supplementary Report.

Response to objectors

The scale of the proposed development is not significant enough to add to the air pollution in Ramsbottom.

The issues relating to a bouncy castle is not a material planning consideration.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

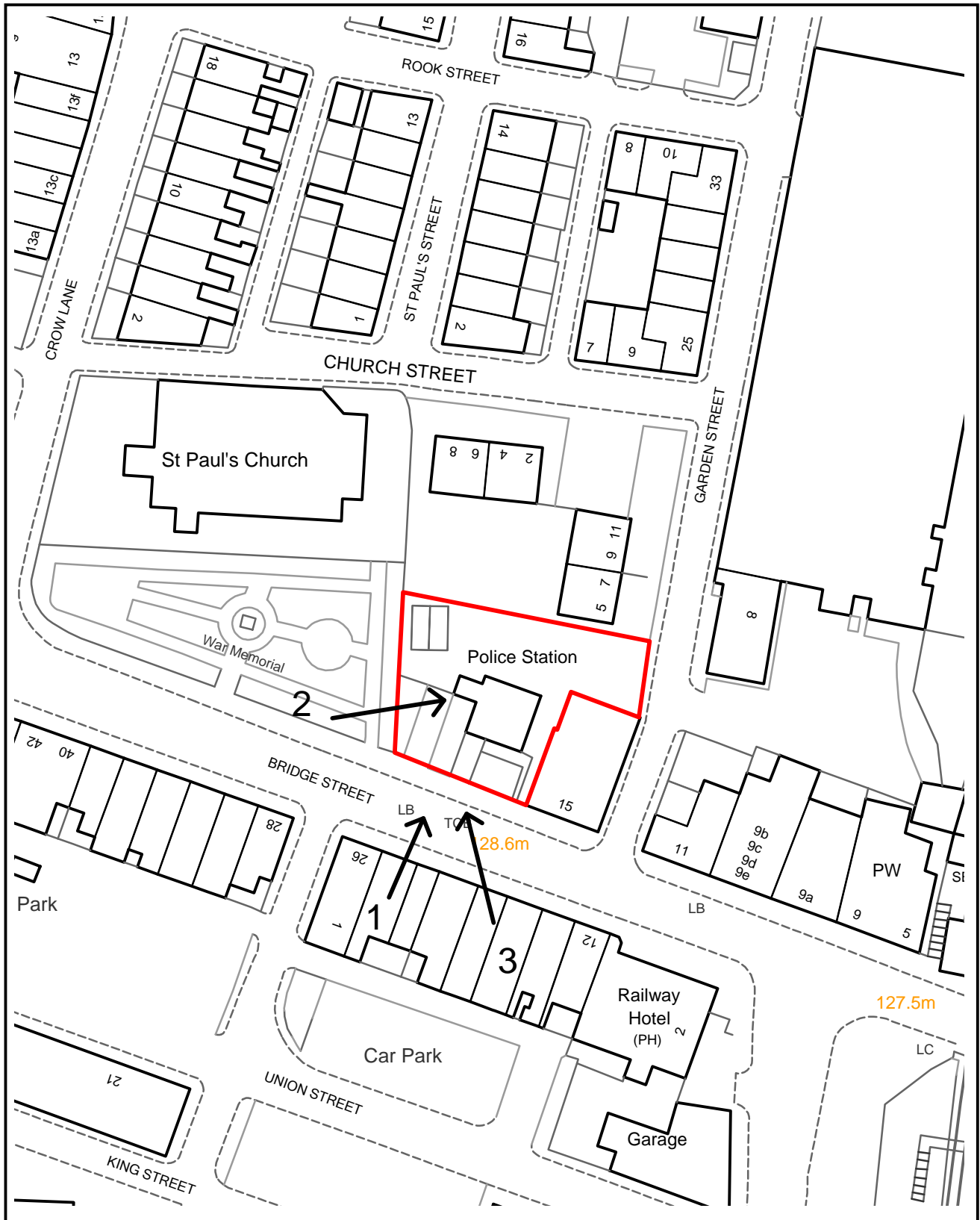
Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered C-0332-SLP, C-0332-01 B and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
3. Details/Samples of the bollards to be used shall be submitted to and approved in writing by the Local Planning Authority before the development is commenced. Only the approved materials/bricks shall be used for the construction of the development.
Reason. No material samples have been submitted and are required in the interests of visual amenity and to ensure a satisfactory development pursuant to UDP Policy EN1/1 Visual Amenity.
4. The car parking indicated on the approved plans shall be surfaced, demarcated and made available for use prior to the use hereby approved being brought into use.
Reason. To ensure adequate off street car parking provision in the interests of road safety pursuant to Policy HT2/4 - Car Parking and New Development of the Bury Unitary Development Plan.

For further information on the application please contact **Helen Longworth** on **0161 253 5322**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 61299

**ADDRESS: Former police station, Bridge Street
Ramsbottom**

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

61299

Photo 1



Photo 2



Photo 3



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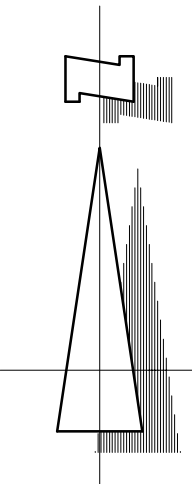
Do not scale from this drawing.

All dimensions are to be checked on site prior to construction, manufacture of any components and ordering of materials and equipment.

Any discrepancies are to be reported to the architect for clarification.

All materials and workmanship to be in accordance with the current British Standards and codes of practice.

This drawing is to be read in conjunction with all relevant Architectural, Structural Engineers, Mechanical Engineers, Electrical Engineers and Specialists drawings and specifications.



B	Highways comments incorporated	MM	12 May 2017
A	Highways comments incorporated	TW	02 May 2017

REV	DESCRIPTION	CHECK	DATE
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CLIENT
MR. ROBERT HODKINSON

PROJECT
Proposed Private Car Park, Former Police Station,
Bridge Street, Ramsbottom, BL0 9AB

DRAWING
Proposed Site Layout

SCALE	DATE	DRAWN	CHECKED
1:100 @ A1	Mar 2017	AL	
DRAWING NO.	CAD REFERENCE NO.	REVISION	
C-0332-02		B	





←
W'stow
Marshes

←
W'stow
Central
Station
Empire
Cinema
Market

BUI2 YXX

Ward: Bury East

Item 08

Applicant: Mr Ali Khan

Location: 60 Bolton Street, Bury, BL9 0LL

Proposal: Change of use from solicitors office (Class A2) to private hire taxi booking office (Sui Generis)

Application Ref: 61300/Full

Target Date: 08/06/2017

Recommendation: Approve with Conditions

Description

The application is retrospective and follows a recent refusal for a similar change of use over a 24hr period. It relates to the ground floor of a former solicitors office on the north side of Bolton Street. The site, covering 90sqm, is situated within a Secondary Shopping Area and Frontage of Bury Town Centre. The site is also within the Town Centre Conservation Area

Castlecroft Mews a block of flats situated around a courtyard is situated to the rear and accessed from a side road next to the site. The attached unit on the west side, is a bathroom shop whilst to the east, across the access road into Castlecroft Mews is a clothes boutique. Across Bolton Street is Castlecroft Leisure Centre.

There are 'pay & display' parking bays on Bolton Street outside the premises with bus stops nearby on both sides of the road. There is a taxi booking office and taxi rank further up Bolton Street, opposite Bolton Street train station.

The proposal is to retain the change the use of the shop unit to a private hire booking office with opening hours from 0600hrs to midnight. The application indicates 4 full time staff and 3 part-time and the plans show a public reception area at the front and an office and kitchen/toilets to the rear.

Relevant Planning History

60806 - Change of use from solicitors office (Class A2) to private hire taxi booking office (Sui Generis) - Refused 12/01/2017

17/0025 - Change of use from solicitors office (class A2) to private hire taxi booking office (Sui generis) - Application received 18/01/2017

Publicity

Immediate neighbours notified by letter dated 23/11/16. Objections received from the Hackney Drivers Association and residents at 1, 8, 9, 11, 17, 21 and 29 Castlecroft Mews, 11 Irwell Street. A separate 16-name petition from the residents at 4,7,8,9,11,17, 19, 21, 27, 29, 31, 33 and 35 Castlecroft Mews objecting to the proposal has also been received.

Concerns are summarised:

- This office is situated opposite the swimming baths. If the parking bays are full, there would be no facilities for parking/picking up customers. It would mean that they may have to double park to pick up any passengers.
- This office is situated close to flats and there would be anti-social behaviour associated with late night activities.
- There maybe issues of public safety during the times of schools attending the swimming baths.
- There are enough booking offices, taxis and private hire vehicles in the area.
- Increase parking problems on the road and cause conflict with occupiers of the nearby

flats.

- Noise and disturbance to nearby residents.
- Increase in litter and other rubbish.

Eight representations have been received in support of the application from businesses at 44, 48, 50b, 54, 62, 64 and 66 Bolton Street.

All those making representations have been notified of the Planning Control Committee.

Consultations

Traffic Section - No objection.

Licensing Department - Objects on the following grounds.

- The proposed change of use of a premises incorporating a waiting area for members of the public is highly likely to lead to private hire vehicles waiting on Bolton Street on existing waiting restrictions or parking bays directly near residential property and an increase in the number of vehicles manoeuvring in a position on the highway network where adequate turning facilities do not exist, resulting in conflicts with pedestrians and vehicles in this busy town centre location, which would be detrimental to highway safety and maintaining the free flow of traffic.
- Complaints are already being received from the Hackney Carriage trade regarding private hire vehicle/private cars parking on the hackney carriage rank in the town centre and are continually dissatisfied with the Council's lack of action over such matters. A Hackney Carriage stand is in close proximity to the premises concerned.
- It is noted that there is a number of residential properties in close proximity of the proposed private hire operator base and should the change of use be approved there may be an increase of complaints to the Council in respect of noise/anti social behaviour.

Environmental Health - No objection received.

Greater Manchester Police - No objection received.

Unitary Development Plan and Policies

Area	Bolton Street/Market Place
BY3	
EN2/1	Character of Conservation Areas
EN2/2	Conservation Area Control
HT2/8	Taxi and Private Hire Businesses
EC4/1	Small Businesses
S2	Control of New Retail and Non-Retail Development
S1/1	Shopping in Bury Town Centre

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Policy - UDP Policy S2/3 - Secondary Shopping Areas and Frontages. Within secondary shopping areas identified in the Borough's town centres, and in the main shopping areas of district centres, the Council will seek to maintain retailing (Class A1) as the predominant land use at ground floor level. Proposals for change of use or redevelopment within these areas will be assessed on their merits and by taking into account the following factors:

- a) the design and appearance of the proposed frontage;
- b) the maintenance or provision of a display window at ground floor level, where appropriate;
- c) access for the mobility impaired, where appropriate and through negotiation with the

developer;

d) whether the proposals will give rise to disturbance or nuisance;

In addition, where a proposal would lead to more than 40% of any identified secondary shopping frontage being in non-retail (Class A1) use, the Council will also take into account the following factors:

e) the location and prominence of the proposal within the secondary shopping frontage;

f) the number, distribution, and proximity of other premises in non-retail (Class A1) use or with planning permission for such uses;

g) the particular nature and character of the use proposed, including the level of activity associated with it.

Policy HT2/8 Taxi and Private Hire Businesses. Proposals for taxi or private hire businesses will be looked upon favourably, providing that the following criteria are satisfied:

a) adequate car parking facilities are provided for the needs of employees and vehicles used in connection with the business;

b) it will not have an unacceptable adverse effect on the amenities of neighbouring residents or occupiers;

c) it will not have an unacceptable adverse effect on the local highway network, in terms of road safety and traffic circulation in particular.

The main planning issues are the effect of the proposed change of use upon the vitality and viability of Bury Town Centre, highway safety and residential amenity.

Impact on Shopping Centre - Bolton Street is a relatively busy road within the town centre and contains a variety shops and retail related uses such as cafes, offices and salons. The booking office has been operating from the premises for a number of months and before this it was a solicitor's office. As a booking office, bookings would be taken by phone and from the reception area when the public walk in to the office from the street.

There would be an active shop front maintained and customers would be going into the building to make the bookings, thereby maintaining a relatively healthy footfall just as there would be for any other office such as its previous use as a solicitors office and as such the proposed use would still make a positive contribution to the streetscape and would be complementary to surrounding town centre uses in terms of the service it provides.

It is not considered that the proposal would have a harmful effect upon the vitality and viability of Bury Town Centre and there would be no conflict with UDP Policy S2/3 which, whilst it seeks to maintain retailing as the predominant land use, recognises that changes of use to non-retail will be assessed on their individual merits. It is important to note that the previous use was as an A2 solicitors office and not an A1 shop.

The proposal is therefore acceptable and, in terms of shopping policy, complies with the NPPF and UDP Policies S1/1, S2/3.

Traffic - In addition to telephone based business, customers visiting the office are mostly likely to be picked up by the private hire vehicles outside, on Bolton Street. This is no different to any other Town Centre businesses, be it shops or offices and it is usual for taxis to drop off and collect passengers on the highway. The presence of the 'Pay & Display' would not prevent cabs stopping and collecting passengers and it is considered possible that taxis collecting fares from the premises would make use of these bays if available. Given that the Local Authority has provided these bays, it must have been accepted that it is safe to stop, park and drive off from them. If taxis were to park up, as opposed to dropping-off and picking-up, this would be a matter for the Council to enforce but it would be wrong to assume that taxi drivers would park illegally any more than normal drivers. It is noted that the parking bays only have restrictions in place for the main daytime period. The parking bays are intended for shoppers and other visitors to the town centre and it is likely that the customers being picked up would include shoppers, thereby maintaining a shoppers use.

Whilst applications are assessed on their individual merits, decisions by the Planning Inspectorate inform Local Planning Authorities on how they interpret policy and situations on the ground at a particular time. A relatively recent appeal decision (57676) to allow a private hire booking office at 431 Bury New Road in Prestwich Town Centre raised issues about the acceptability of booking offices without dedicated off-street parking or unrestricted on-street parking. The inspector argued that a town centre booking office was acceptable even where there was no dedicated parking and that restricted parking/waiting areas were acceptable for picking-up and dropping-off of passengers. A recent application for a booking office was approved at another town centre location, on Tithebarn Street, on this basis.

On balance, it is not considered that the proposed change of use would not have a significantly harmful effect upon highway safety and there would be no conflict with UDP Policies EC4/1, HT2/4, HT2/8 and HT6/2 in relation to parking and highway and pedestrian safety.

Residential Amenity - The location of a 24-hour private hire booking office on Bolton Street, immediately adjacent to the entrance to flats at Castlecroft Mews raises residential amenity concerns and this was reflected in the decision to refuse a previous application for the change of use to booking office that would have opened on a 24-hour basis. The reason for refusal centred on the noise and disturbance caused to nearby residents by queuing customers waiting for a pick-up along Bolton Road, particularly into the early hours at weekends.

This proposal has revised the opening hours which now extend until midnight. Whilst there may still be some noise created on Bolton Road by customers, the more excessive noise and disturbance occurring into the early hours would be avoided. The proposal, given the limited opening hours is considered to be acceptable and complies with UDP Policies EC4/4 Small Businesses, EN7/2 Noise Pollution and HT2/8 Taxi and Private Hire Businesses.

Character of the Conservation Area - Given that there are no significant external alterations other than signage (which is subject to separate planning legislation) the impact on the character of the Town Centre Conservation Area is negligible. The change of use of the premises would preserve the character of the conservation area and would comply with Section 12 of the NPPF and UDP Policies EN2/1 and EN2/2 relating to conservation areas.

Representations - The issue with regard to private hire vehicles waiting on the parking bays or other restricted areas would be a matter for the Council's Parking Enforcement.

With regard to vehicle/pedestrian/resident conflict, should the existing traffic regulations be observed, conflict would be minimised and as such this would not be a particularly robust reason to refuse the proposal.

The concerns relating to residential amenity/ noise and disturbance have been addressed in the above report and are mitigated somewhat by the condition restricting the hours of opening.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

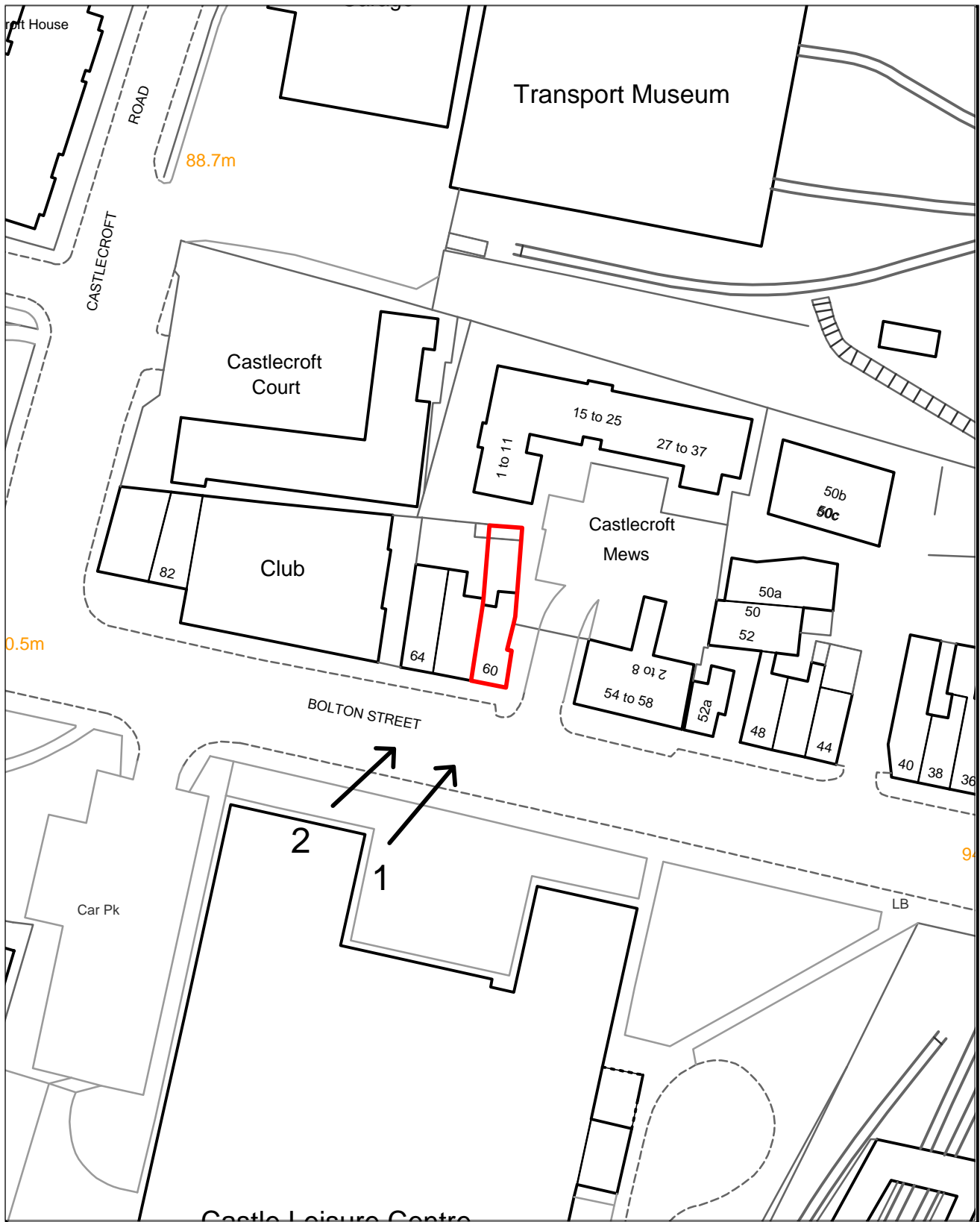
Recommendation: Approve with Conditions

Conditions/ Reasons

1. This decision relates to location and site plans, layout 01 and elevations 02. The development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.
2. The booking office hereby approved shall not be open to customers outside the following hours:
0600hrs to midnight 0000hrs daily.
Reason. To safeguard the amenities of the occupiers of nearby residential accommodation pursuant to UDP Policies EC4/1 Small Businesses and HT2/8 Taxi and Private Hire Businesses.

For further information on the application please contact **Tom Beirne** on **0161 253 5361**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 61300

**ADDRESS: 60 Bolton Street
Bury**

Planning, Environmental and Regulatory Services

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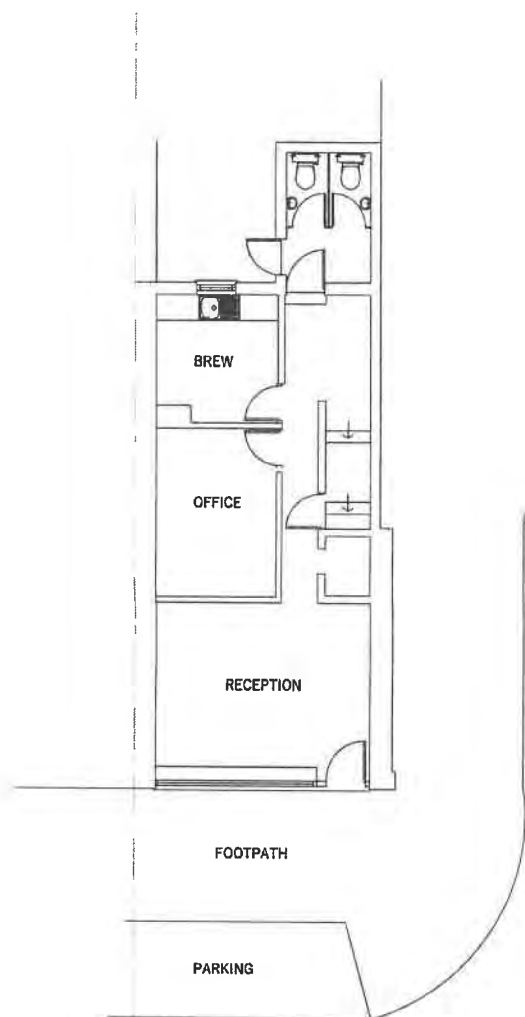
61300

Photo 1

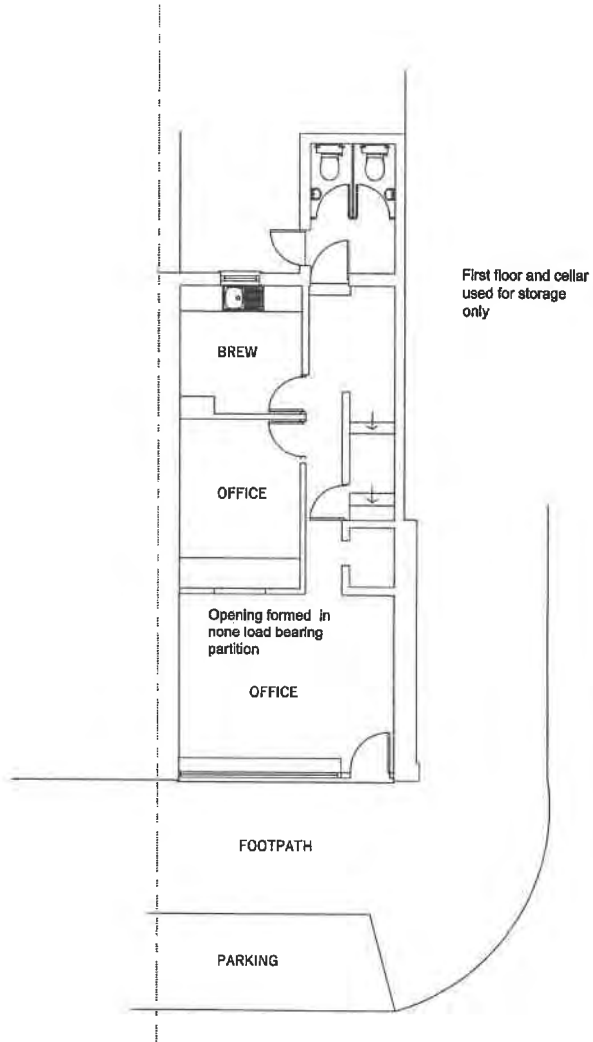


Photo 2





EXISTING PLAN

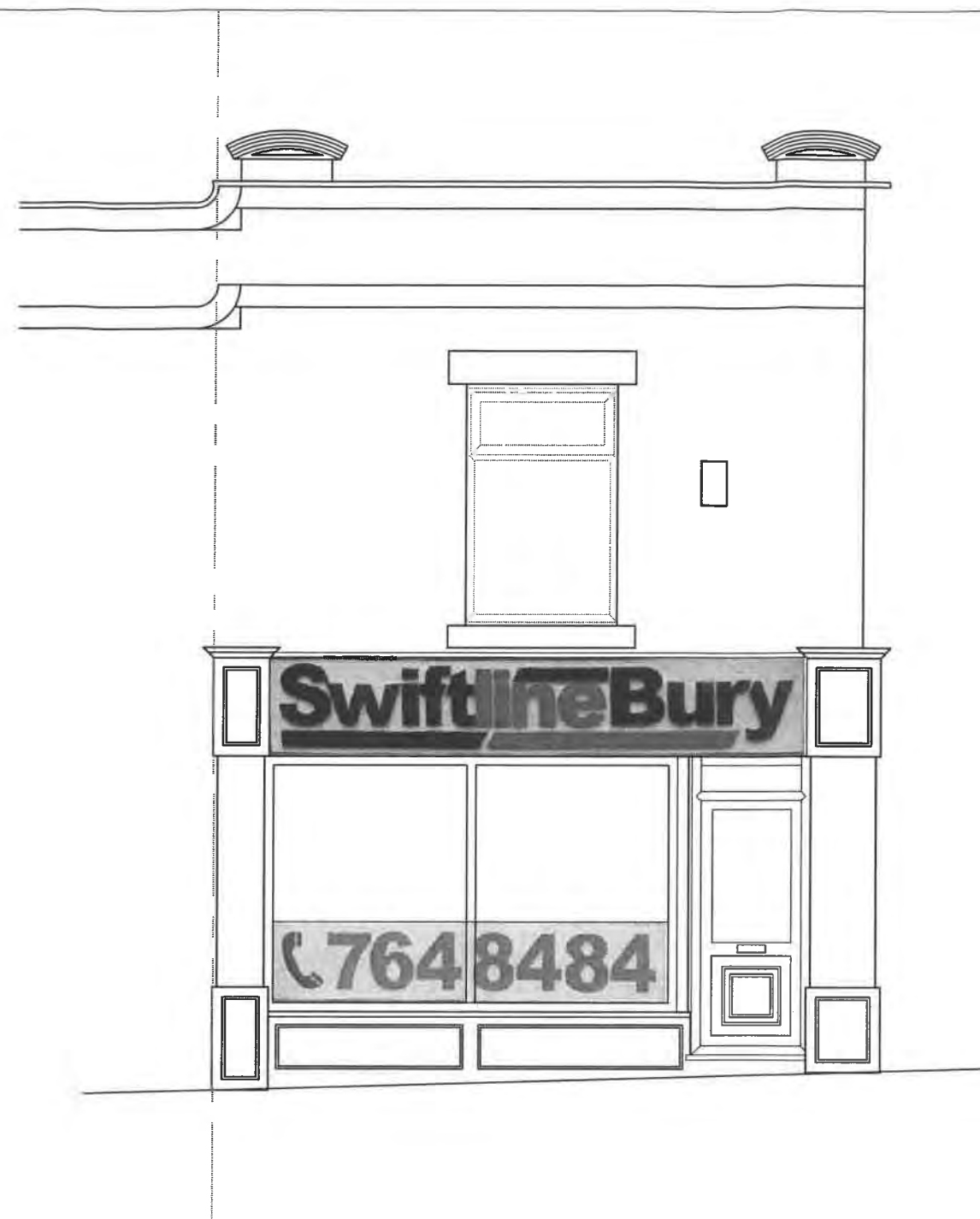


PROPOSED PLAN

REV	DATE	AMENDMENTS	INT
Harry Jackson Surveyors Limited Unit 4A Farnworth Park Industrial Estate Wellington Street, Farnworth, Bolton BL4 7BY Tel: 01204 701740 Mobile: 07931 321878			
CLIENT		MR G ALI KHAN	
ADDRESS		60 BOLTON STREET BURY BL9 0LL	
PROJECT		PRIVATE HIRE VEHICLES OFFICE & CALL CENTRE	
TITLE		EXISTING AND PROPOSED PLANS	
DRAWING No.	01	REV	0
SCALES	1:100	DATE	March 2017
DRAWN BY	BE	SHEET SIZE	A3
CHECKED BY	HJ	COPYRIGHT	©



EXISTING FRONT ELEVATION



PROPOSED FRONT ELEVATION

REV	DATE	AMENDMENTS	INT
Harry Jackson Surveyors Limited Unit 4A Farnworth Park Industrial Estate Wellington Street, Farnworth, Bolton BL4 7BY Tel: 01204 701740 Mobile: 07931 321879			
CLIENT	MR G ALI KHAN		
ADDRESS	60 BOLTON STREET BURY BL9 0LL		
PROJECT	PRIVATE HIRE VEHICLES OFFICE & CALL CENTRE		
TITLE	EXS. & PROP FRONT ELEVATION		
DRAWING No.	02	REV	0
SCALES	1:100	DATE	March 2017
DRAWN BY	BE	SHEET SIZE	A3
CHECKED BY	HJ	COPYRIGHT	©

Ward: Whitefield + Unsworth - Besses

Item 09

Applicant: Mr N Bayley

Location: 17 Beverley Close, Whitefield, Manchester, M45 8BB

Proposal: Single storey rear extension

Application Ref: 61320/Full

Target Date: 25/05/2017

Recommendation: Approve with Conditions

Description

This application is a householder development and would normally be dealt with under delegated powers. It is presented to the committee as the applicant is Councillor Noel Bayley.

The property is a mid town house located in a row of 4 and located on a residential estate. It has a fenced garden to the rear which is accessed via a gate to the rear of the adjacent garden to No.15. No.19 is the neighbouring property to the north side and the rear elevation has a window to the boundary side and a door adjacent. No.15 to the opposite side has its back door closest to the boundary.

To the rear the garden borders Hillock Hebrew Congregation building.

The application proposes a single storey extension 3m deep at the rear to form a dining room. The roof would be a monopitch to a height of 4m with eaves at 3m. An extension of this size would normally be permitted development however these rights were removed by condition under the approval for the development in 1993. At this time permitted development allowed extensions up to 70 cubic metres. This has now changed to limitations to the depth and height and which the proposal would fall within.

Relevant Planning History

28564/93 - Residential Development - 20 dwellings - AC 12/08/93

26725/91 - Outline residential development - AC 23/01/92

26690 - Outline residential development - AC 23/01/92.

Publicity

3 notification letters were sent on 30/03/17 to addresses at 15 & 19 Beverley Close and Hillock Synagogue. No responses have been received.

Consultations

None.

Unitary Development Plan and Policies

H2/3 Extensions and Alterations

SPD6 Supplementary Planning Document 6: Alterations & Extensions

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless

there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Visual amenity and design - The extension would have a monopitch roof with 2 roof lights and patio doors to the rear elevation. Materials would be roof tiles and brickwork to match the existing house.

The proposal is considered to be acceptable and in keeping with the existing property.

Residential amenity - The extension would run across the whole of the rear elevation and along the boundary with the neighbouring properties. Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties states that single storey extensions should be assessed as to their impact on neighbouring properties where in excess of 3m. The proposed extension at 3m deep would then be within these limits. The side walls would be blank wall and as such there would be no concerns regarding overlooking.

The proposal complies with UDP Policy H2/3 and SPD6 - Alterations and Extensions to Residential Properties.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The proposal complies with the development plan and would improve the economic, social and environmental conditions of the area. It therefore comprises sustainable development and the Local Planning Authority worked proactively and positively to issue the decision without delay. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

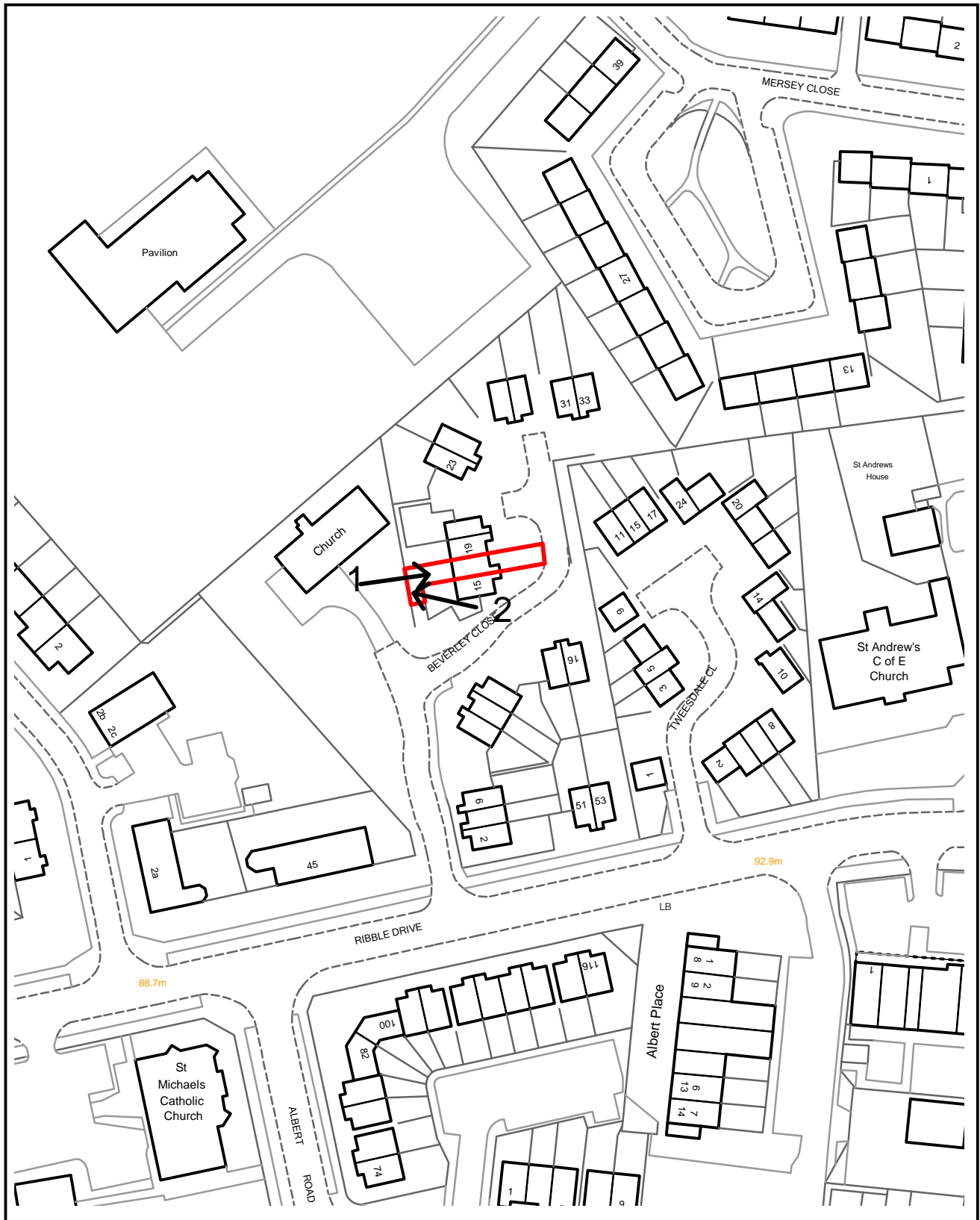
Recommendation: Approve with Conditions

Conditions/ Reasons

1. The development must be begun not later than three years beginning with the date of this permission.
Reason. Required to be imposed by Section 91 Town & Country Planning Act 1990.
2. This decision relates to drawings numbered 17BC/1A and the development shall not be carried out except in accordance with the drawings hereby approved.
Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

For further information on the application please contact **Jane Langan** on **0161 253 5316**

Viewpoints



PLANNING APPLICATION LOCATION PLAN

APP. NO 61320

**ADDRESS: 17 Beverley Close
Whitefield**

Planning, Environmental and Regulatory Services

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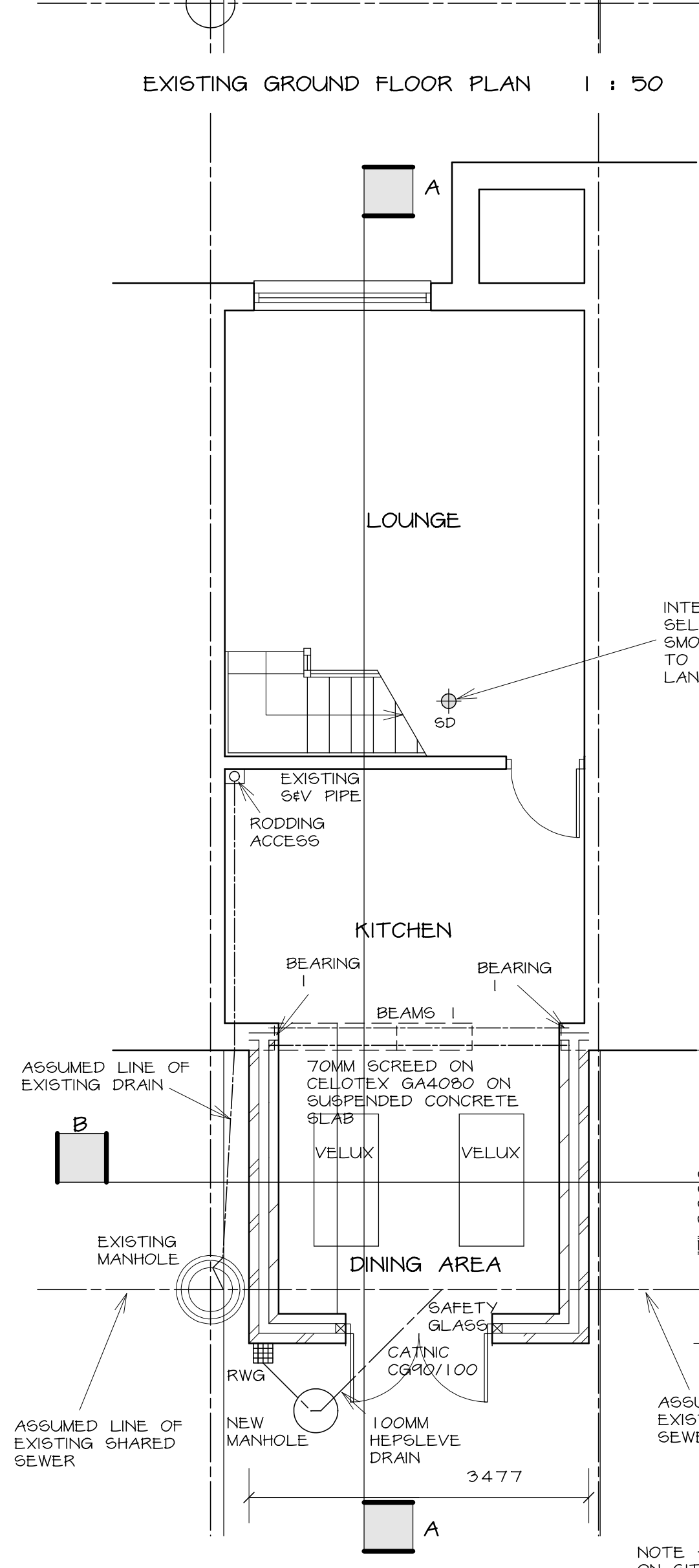
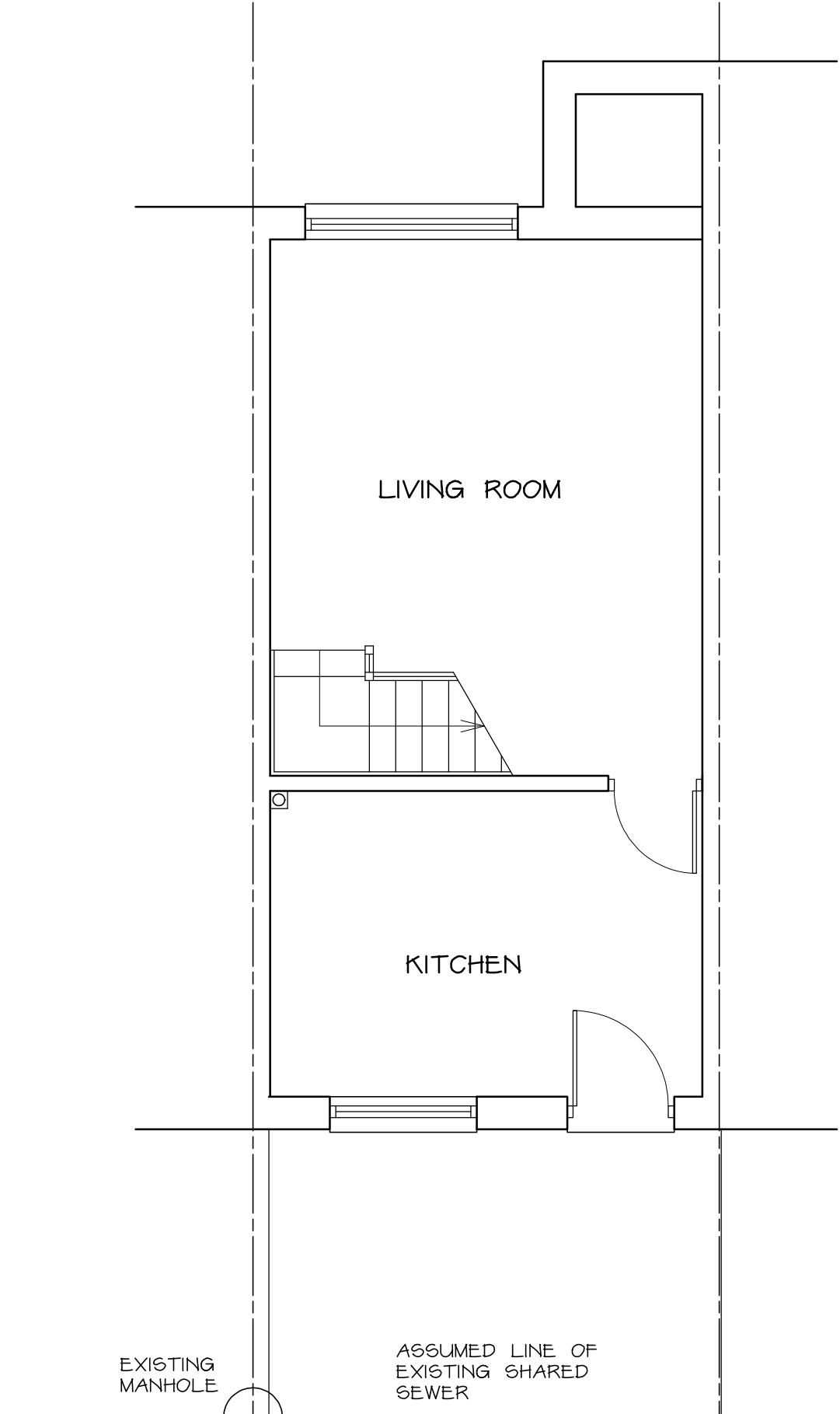
61320

Photo 1

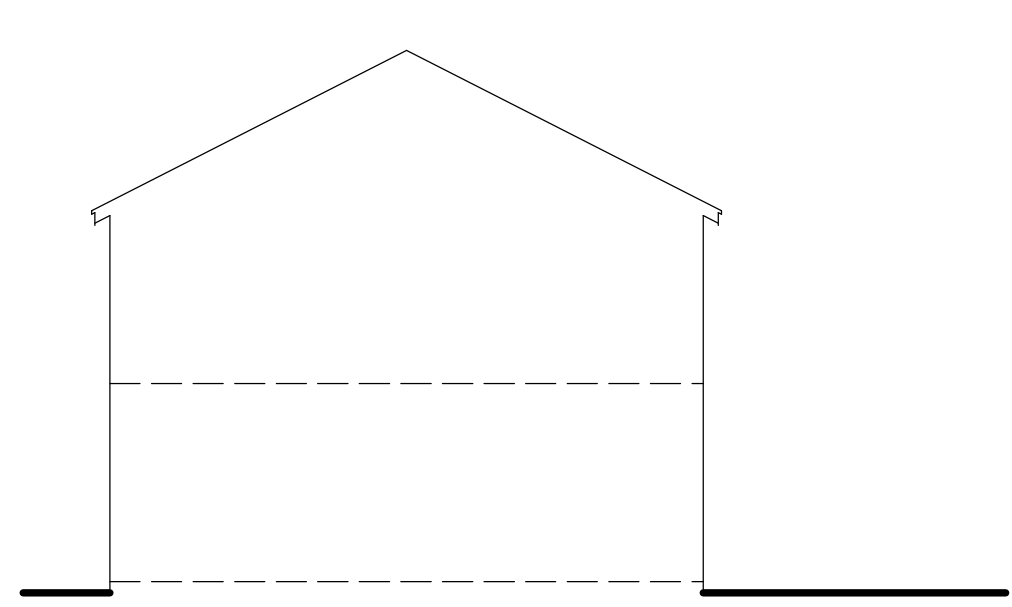


Photo 2

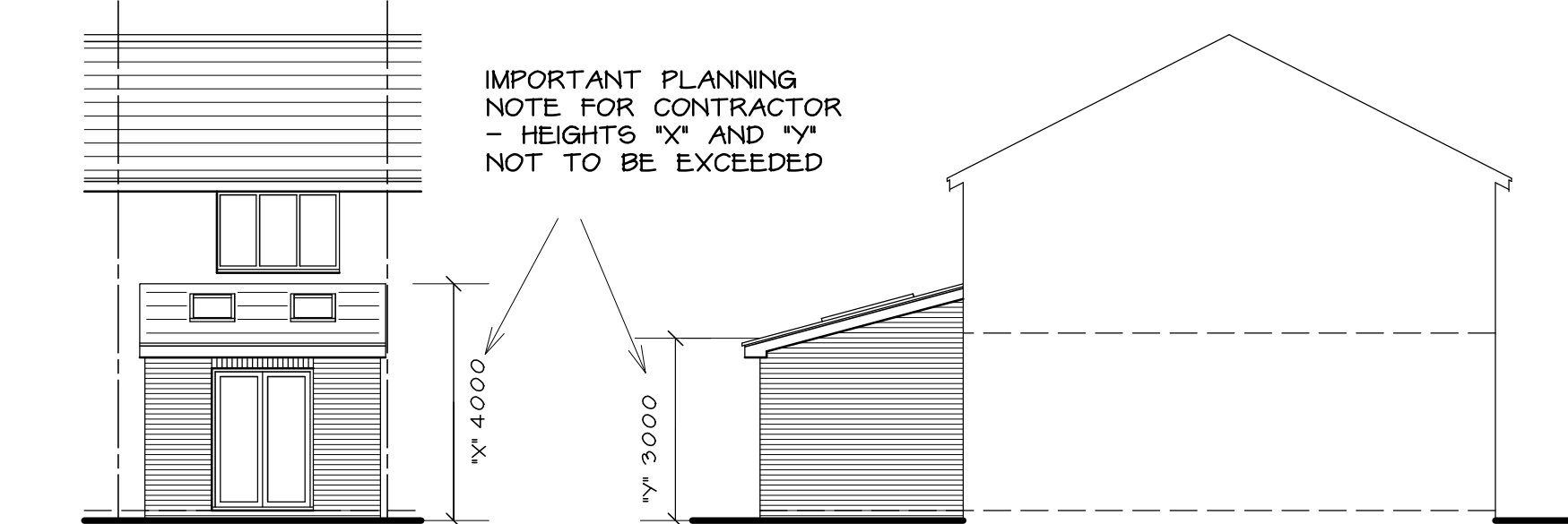
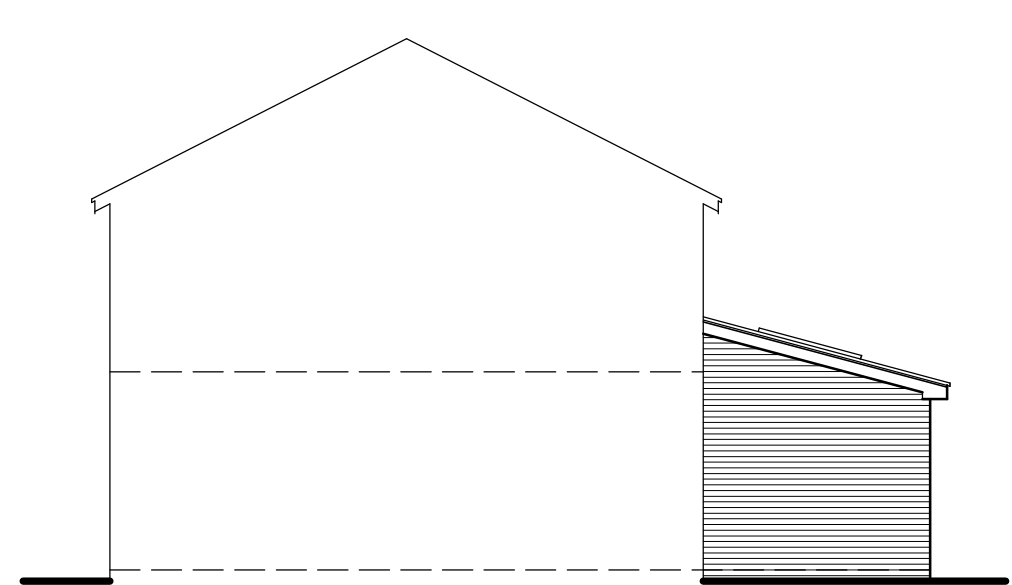




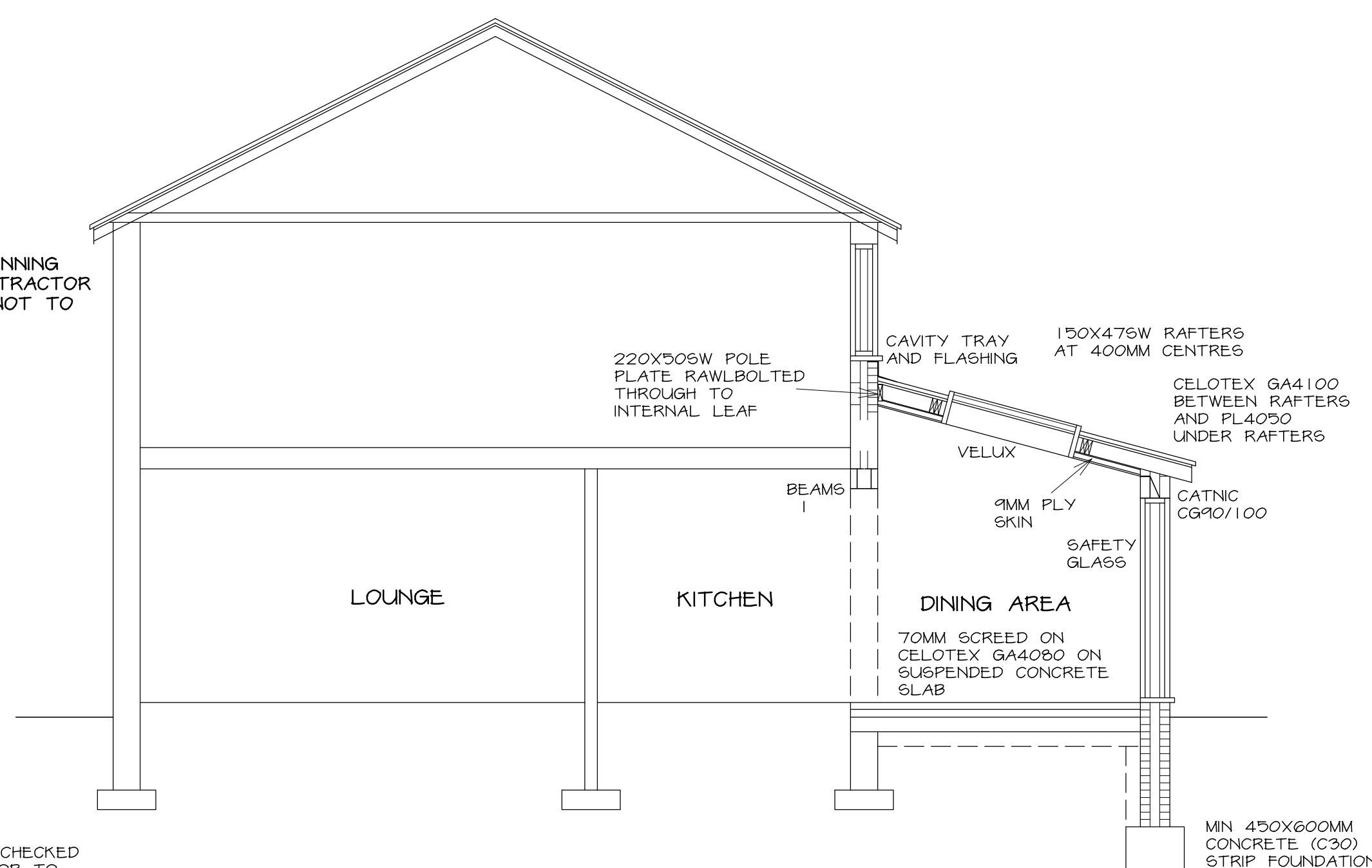
PROPOSED GROUND FLOOR PLAN 1 : 50



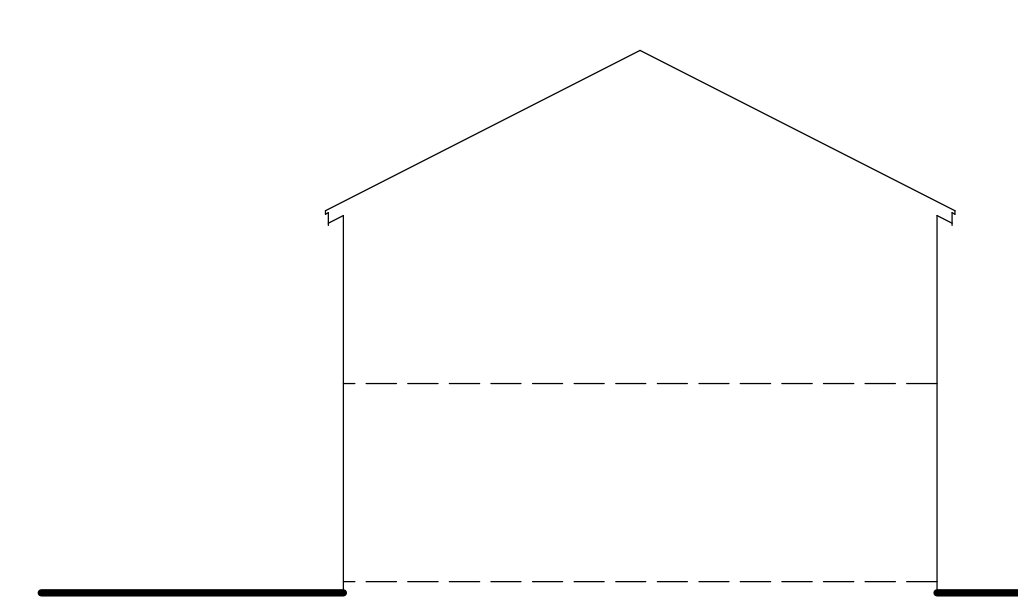
EXISTING ELEVATIONS 1 : 100



PROPOSED ELEVATIONS 1 : 100



SECTION A - A 1 : 50

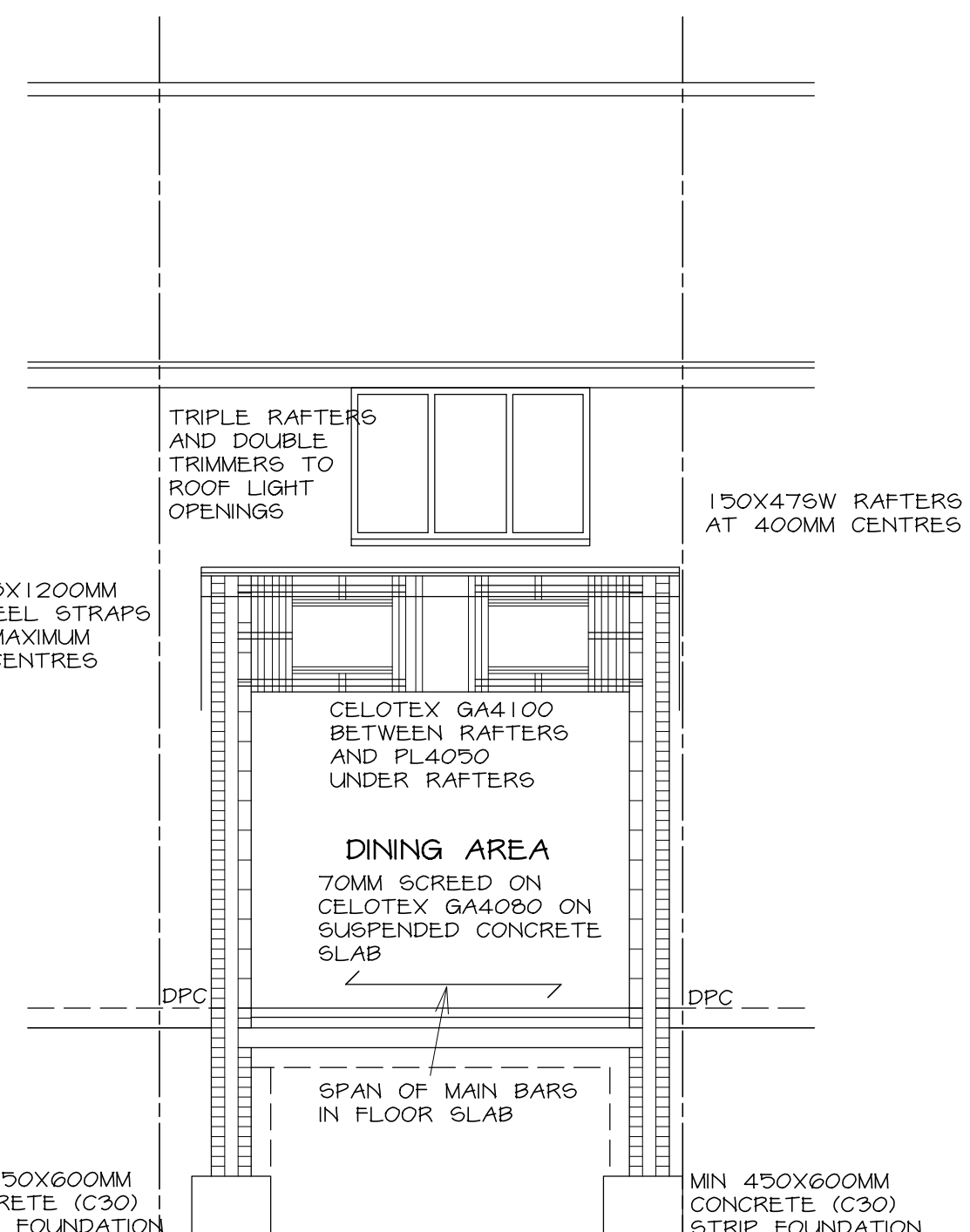


VENTILATION - WINDOWS TO OPEN MORE THAN 30° AND THE OPENINGS TO BE THE EQUIVALENT TO MINIMUM 1/20TH ROOM FLOOR AREA. MECHANICAL VENT TO KITCHEN (60L/SEC). 5000MM² TRICKLE VENTS TO HABITABLE ROOMS.

HEATING AND HOT WATER INSTALLATION - ANY NEW/REPLACEMENT NATURAL GAS AND LPG BOILERS TO HAVE MINIMUM 90% SEDBUCK RATING. OIL FIRED BOILERS TO HAVE MINIMUM 90% SEDBUCK RATING. EXCEPTIONAL CIRCUMSTANCES PERMITTING THE INSTALLATION OF A NON-CONDENSING BOILER - THE INSTALLER TO COMPLETE AN 'AGREEMENT FORM' USING THE PROCEDURE DESCRIBED IN THE DOCUMENT 'GUIDE TO THE CONDENSING BOILER INSTALLATION PROCEDURE FOR DWELLINGS' (ODPM 2009). THE DECLARATION TO BE RETAINED BY THE OWNER AS IT MAY BE NEEDED WHEN THE PROPERTY IS OFFERED FOR SALE. CONDENSING TYPE BOILERS TO HAVE ALL SAFETY PIPES AND CONDENSATE DRAINS DRAINING TO GULLEYS. BOILER FLUE OUTLETS TO EXTEND MINIMUM 200MM BEYOND THE EAVES. THERMOSTATIC AND ZONE CONTROLS TO EXTENDED HEATING INSTALLATION. OPERATING AND MAINTENANCE MANUALS FOR SERVICES TO BE PROVIDED TO THE HOME OWNER.

ELECTRICAL - ALL WIRING AND ELECTRICAL WORK TO BE DESIGNED, INSTALLED, INSPECTED AND TESTED IN ACCORDANCE WITH THE REQUIREMENTS OF BS7671, THE IEE 17TH EDITION WIRING GUIDANCE AND BUILDING REGULATION PART P (ELECTRICAL SAFETY) BY A COMPETENT PERSON REGISTERED WITH AN ELECTRICAL SELF CERTIFICATION BODY AUTHORISED BY THE SECRETARY OF STATE. THE COMPETENT PERSON IS TO SEND TO THE LOCAL AUTHORITY A 'SELF CERTIFICATION CERTIFICATE' WITHIN 30 DAYS OF THE COMPLETION OF THE ELECTRICAL WORKS. THE CLIENT TO BE PROVIDED WITH A COPY OF THE 'SELF CERTIFICATION CERTIFICATE' AND A BS7671 ELECTRICAL INSTALLATION TEST CERTIFICATE. 3 IN 4 LIGHT FITTINGS IN DIFFERENT LOCATIONS TO ONLY ACCEPT LAMPS WITH LUMINOUS EFFICIACY GREATER THAN 45 LUMENS/CIRCUIT WATT.

PLUMBING AND DRAINAGE - AS THE FIRST OPTION RAINWATER TO DISCHARGE TO A STONE SOAKAWAY SITUATED MINIMUM 5M AWAY FROM ANY BUILDING BUT SUBJECT TO A SATISFACTORY PERCOLATION TEST. 110MM UPVC RW GUTTER ON 15MM SW FASCIA DISCHARGING TO RW GULLEY VIA 75MM UPVC RW PIPE. 75MM UPVC TRAPS AND 50MM DIA. WASTES TO WASTE APPLIANCES. 100MM HEPGLEVE DRAINS - MINIMUM 1 IN 40 FALL AND NEW HEPSEAL MANHOLE. THE DRAINAGE SHOWN IS PROVISIONAL AND SUBJECT TO AGREEMENT ON SITE WITH THE BUILDING INSPECTOR ONCE THE EXISTING DRAINAGE HAS BEEN EXPOSED. DRAINS UNDER EXTENSION TO BE ENCASED IN CONCRETE (C20) OR SURROUNDED IN GRANULAR MATERIAL IF FLEXIBLE JOINTED PIPES. FOUNDATIONS TO BE BELOW INVERT. PRECAST CONCRETE LINELS TO SUPPORT FOUNDATION BRICKWORK OVER DRAINS. RODDING ACCESS TO BE PROVIDED.



SECTION B - B 1 : 50

ROOF - REDLAND REGENT SMOOTH CONCRETE ROOF TILES TO MATCH EXISTING - MINIMUM 15° PITCH WITH 100MM HEADLAP - ON 50X256W BATTENS AND TYVEK SUPRO BREATHABLE MEMBRANE ON 150X476W RAFTERS AT 400MM CENTRES. RAFTERS SECURED TO POLE PLATE AND WALL PLATE WITH TRUGGS CLIPS. TRIPLE RAFTERS AND DOUBLE TRIMMERS TO ROOF LIGHT OPENING. 9MM EXTERIOR QUALITY PLY SKIN NAILED AT MAXIMUM 400MM CENTRES TO UNDERSIDE OF RAFTERS. CAVITY TRAYS AND FLASHINGS AT ABUTMENT WITH EXTERNAL WALLS. 100X506W WALL PLATE STRAPPED TO WALLS WITH 30X5MM GALVANISED MSTEEL STRAPS AT MAXIMUM 1.2M CENTRES. 220X506W POLE PLATE RAWLBOLTED THROUGH TO EXISTING INNER LEAF AT MAXIMUM 900MM CENTRES. CELOTEX GA4100 INSULATION BETWEEN RAFTERS AND CELOTEX GA4080 UNDER PLY SKIN ALL AS AGREEMENT CERTIFICATE AND PLASTER SKIM FINISH. 30X5X1200MM GALVANISED MSTEEL RESTRAINT STRAPS TO GABLE WALLS AT MAXIMUM 2M CENTRES INCLUDING SW NOGGING BETWEEN RAFTERS. NOTE - RECESSED DOWNLIGHTERS TO ONLY BE FITTED INTO FALSE CEILING BELOW STRUCTURAL CEILING TO AVOID PENETRATION OF INSULATION.

EXTERNAL WALLS - 103MM OUTER LEAF BRICKWORK TO MATCH EXISTING. 100MM CAVITY WITH GNO. STAINLESS STEEL TIES/M² AND 100MM DRITHERM CAVITY SLAB PLUS INSULATION AS AGREEMENT CERTIFICATE TAKEN DOWN 225MM BELOW HORIZONTAL DPC - 100MM INNER LEAF THERMALITE BLOCK (3.6N) WITH 12.5MM PLASTERBOARD AND SKIM LINING ON PLASTER DABS. HORIZONTAL DPCS - MINIMUM 150MM ABOVE GROUND LEVEL. VERTICAL DPCS AND CAVITY TRAYS TO ALL NEW OPENINGS. NEW WALLS BONDED TO EXISTING AND CELOTEX GA4080 INSULATION TO STEEL LINELS IN EXTERNAL WALLS AND THERMABATE CAVITY CLOSERS.

GROUND FLOOR - 25MM CELOTEX AROUND PERIMETER OF 70MM SCREED ON 500G VISQUEEN ON CELOTEX GA4080 INSULATION AS AGREEMENT CERTIFICATE ON 1200G VISQUEEN DPM - ON REINFORCED CONCRETE SUSPENDED SLAB (DESIGN AND CALCULATIONS FOR SLAB TO BE PREPARED BY A STRUCTURAL ENGINEER AND SUBMITTED TO THE BUILDING INSPECTOR FOR APPROVAL PRIOR TO COMMENCEMENT OF WORK ON SITE) - ON 1200G VISQUEEN - TURNED UP AT PERIMETER TO LAP HORIZONTAL DPCS - ON MINIMUM 150MM CONSOLIDATED STONE HARDCORE BLINDED WITH SAND. ANY EXISTING SUB FLOOR VENTS TO BE PIPED THROUGH TO NEW EXTERNAL WALL.

FOUNDATIONS - MINIMUM 600X450MM CONCRETE (C30) STRIP FOUNDATIONS - DEPTH AS REG. A1/2 - MINIMUM 900MM BELOW GROUND LEVEL. 305MM FOUNDATION CAVITY BRICKWORK IN COMMONS WITH CAVITY FILL TO GROUND LEVEL. NOTE FOUNDATION SPECIFICATION IS PROVISIONAL AND SUBJECT TO ADEQUATE GROUND BEARING. NOTES CONCRETE FOUNDATIONS TO BE OF SUFFICIENT DEPTH TO ALLOW FOR THE ASSUMED SHARED SEWER TO PASS THROUGH FOUNDATION WALL. IT IS RECOMMENDED THAT A TRIAL HOLE INVESTIGATION IS CARRIED OUT PRIOR TO COMMENCEMENT OF WORK ON SITE TO ESTABLISH THE EXISTING GROUND BEARING CONDITIONS AND THE EXISTING FOUNDATION TYPE AS THIS COULD AFFECT THE PROXIMITY OF THE FLANKING WALL TO THE EXISTING DRAINAGE INCLUDING THE EXISTING MANHOLE.

GENERAL - STRUCTURAL TIMBER (C16 GRADE) AND EXTERNAL TIMBER TREATED WITH PRESERVATIVE. STEEL TO BEAR 150MM AND CLAD WITH 19MM PLASTERBOARD AND SKIM (1/2HR FR5). DOUBLE GLAZED DOORS AND ROOFLIGHTS (1.8W/M²C) WITH MINIMUM 16MM AIRGAP AND 60FT LOW-E COATING. SAFETY GLASS TO EXTERNAL DOORS AND ANY ATTACHED WINDOWS. INTERCONNECTED SELF CONTAINED SMOKE ALARMS (WITH BATTERY BACK UP) TO LOUNGE AND LANDING PERMANENTLY WIRED TO A SEPARATELY FUSED CIRCUIT AT THE DISTRIBUTION BOARD IN ACCORDANCE WITH BS5839 - 62004. STRUCTURAL DESIGN AND CALCULATIONS FOR SUSPENDED FLOOR SLAB, BEAMS AND BEARINGS TO BE PREPARED BY STRUCTURAL ENGINEER AND SUBMITTED TO THE BUILDING INSPECTOR PRIOR TO COMMENCEMENT OF WORK ON SITE. EXISTING STRUCTURE AND FOUNDATIONS TO BE EXPOSED FOR INSPECTION AS REQUIRED BY THE BUILDING INSPECTOR TO CONFIRM ADEQUACY FOR NEW LOADINGS PRIOR TO COMMENCEMENT OF WORK ON SITE. ROBUST CONSTRUCTION DETAILS TO BE INCORPORATED. DIMENSIONS FOR ORDERING MATERIALS OR FOR MANUFACTURING COMPONENTS TO BE OBTAINED BY MEASUREMENT ON SITE BY THE CONTRACTOR.

Telephone 0161 643 2422

Northside Plans Ltd

Planning and Building Design

116 Simister Lane Prestwich Manchester M25 2SB

Project

SINGLE STOREY REAR EXTENSION
17 BEVERLEY CLOSE
WHITEFIELD

Title

PROPOSED PLANS

Scale 1:50 1:100 1:200 (A1) Date 03/2017
Drawn by NIGEL TAM6 Dwg.no. 17BC/1A

Ward: Bury East - Redvales

Item 10

Applicant: Mr Graham Philips

Location: Land adjacent to 23 Meadway, Bury, BL9 9TY

Proposal: Outline application for 2 no. detached dwellings with detached double garages with details of access.

Application Ref: 61369/Outline Planning
Permission

Target Date: 07/06/2017

Recommendation: Approve with Conditions

Description

The application site relates to a piece of grassed open land which is situated within a cul de sac of a residential development comprising 5 detached properties. The eastern part of the site is identified as being located in the River Valley (UDP Policy OL5/2) and Wildlife Corridor (UDP Policy EN6/4) and shares the boundary with Roch Valley Greenway which is Protected Recreation under UDP Policy RT1/1.

To the east is a raised bank along which is a line of trees and vegetation. To the north is planting to the boundary with No 23 Meadway and their detached garage and driveway. To the south and west the site is partly bounded by hedging and a row of leylandii trees. The site appears to be well maintained and mowed regularly.

The existing houses on the cul de sac are located to the west of the application site. The cul de sac is unadopted but the rest of Meadway which serves the other residential properties to the south of the entrance to the cul de sac is adopted.

There is a public sewer which runs through the site from north to south.

The proposal comprises the residential development for two detached properties with double garages. The application is in outline, with access details only applied for. Matters relating to appearance, layout, scale and landscaping would be sought through the submission of a Reserved Matters application.

The existing cul de sac access would be utilised and the proposal includes a passing place on the bend close to the entrance. Indicative plans show that the properties would face Meadway with the driveways located in front and their access directly taken from the cul de sac, similar to the existing houses.

The application proposals indicate that only a part of the site is within the Applicant's ownership, the remaining area being unknown. Certification has been signed to declare the Applicant does not own the entire application site and the application has been advertised as required by legislation in the local newspaper in accordance with publicity requirements.

Relevant Planning History

01872/E - Pre-application enquiry for proposed No.2 detached houses & No.2 detached double garages - Enquiry completed 18/08/2016

60786 - Outline application for residential development of 2 no. detached dwellings with details of access (all other matters reserved) - Withdrawn by Applicant 15/02/2017

Publicity

15 letters sent on 12/4/2017 to properties at Nos 3-21 (odds) and 38-48 (evens) Meadway. Site notice posted 20/4/2017.

Seven letters of objection received from Nos 15, 17, 19, 21, 42, 44, 46 Meadway which raise the following issues:

- When the houses were originally built, there was considerable damage caused by wagons, together with their noise;
- Japanese knotweed - The Ecological Impact assessment states there is Japanese knotweed in the adjacent area creating a serious spreading risk. There is also evidence of Japanese knotweed along the banks of the river behind the properties Nos 1-48 Meadway. This is difficult to treat and access as it is on the sloping bank. The risk of spread is too great to allow development of the land.
- Road into cul de sac - The entrance road to the development is unadopted and has shared ownership by Nos 15-23 Meadway with each household owning a particular section and responsible for the maintenance. The passing place would not meet current road safety standards and would require the removal of the street lighting. There is no pavement. Increased risk to cyclists and pedestrians or to children playing on Meadway. There would be no agreement to changes in the road way or lighting removal. Entrance walls and conifer hedging are maintained and cost shared by the residents. Would not give consent to any building contractors and construction vehicles in relation to the development. Traffic and machinery would damage the road. Road safety issues at the entrance would not be resolved. To build 2 large houses where there is likely to be an extra 4 cars at least will add to traffic, leaving at speed, in and out of Meadway; Would restrict access to our driveways. The traffic to the cul de sac is already bad and 2 more homes will create a lot more traffic.
- Land ownership - The land the houses are proposed to be built on is not wholly owned by the applicant. Discrepancy between submitted plan and HM Land Registry plan which shows significantly different areas of land owned by the applicant. The Land Registry plan also shows the garage to No 23 erected on land outside the boundary. The land to the front of No 23 is NOT within the applicant's garden boundary. The land has been treated as communal - grass cutting does not constitute ownership. It is a piece of common land as it is unregistered. If it is claimed to be the 'garden to No 23' this would contravene H2/6 - Garden and Backland Development.
- Flood Plain - It is clear how close the development is to the River Roch and the land is on the flood plain of the River Roch. This area was seriously flooded in December 2015. The cycle path was flooded all the way to Goshen playing fields as a result of the flooding. The sewer was unable to cope with the volume of water. Any new build would be at serious risk of flooding and could lead to flooding for the existing properties. A flood report should be commissioned. The NPPF para 94 states that LPA's should adopt proactive strategies to mitigate and adapt to climate change taking account of flood risk. Two further properties would place increased strain on the system.
- Main sewer and electric cables - A main sewer crosses the proposed site. Having consulted with UU there would need to be at least a 5m easement either side of the sewer pipe pushing the houses back than are proposed. The house would require piling which could damage the pipeline or underground electric cables. When the original houses were built in 1992 the builder was refused permission to build on the land because of the position of the sewer. The Land Registry Document states that covenants must keep the easement land open and unbuilt on, and no alterations made to levels of the land.
- Cramming - Dispute the proposed dwellings are 'in keeping with the surroundings'. It would result in cramming and over development. The gardens would be small, little space for landscaping. Loss of valuable green space. Overlooking, impact on privacy, noise and disturbance issues to existing residents. Design looks ugly and the scale and size neither appropriate to or sympathetic with the character of the street scene. Noise, pollution and dust from additional cars.
- Wildlife - Additional land from the River valley and Wildlife Corridor would be needed for rear gardens and trees which is not owned by the applicant. Would have a serious impact on wildlife and natural habitat. Removal of trees and shrubs not owned by the applicant should not be permitted.

- **Coal Authority** - A search appears to confirm the area is in a 'high risk' area and a mining report recommended. Would advise you order an Enviro All-in-One report.

Photographs of the area previously flooded and Land Ownership Registry Documents have been submitted with the objections.

The objectors have been informed of the Planning Control Committee meeting.

Consultations

Traffic Section - No objection subject to conditions.

Drainage Section - Standard condition included.

Environmental Health Contaminated Land - No objection subject to conditions.

Environment Agency - No comments to make. The majority of the site is in Flood Zone 1 and a Flood Risk Assessment is not required.

United Utilities (Water and Waste) - No objection subject to conditions.

The Coal Authority - No objection subject to conditions.

Greater Manchester Ecology Unit - No objection subject to conditions.

Public Rights of Way Officer - Public Right of Way No. 68, Bury abuts the shared access to the site. The development should not affect this footpath.

Unitary Development Plan and Policies

NPPF	National Planning Policy Framework
H1/2	Further Housing Development
H2/1	The Form of New Residential Development
H2/2	The Layout of New Residential Development
H2/6	Garden and Backland Development
EN1/2	Townscape and Built Design
EN6/4	Wildlife Links and Corridors
EN5/1	New Development and Flood Risk
OL5/2	Development in River Valleys
HT2/4	Car Parking and New Development
HT6/2	Pedestrian/Vehicular Conflict
RT1/1	Protection of Recreation Provision in the Urban Area
SPD11	Parking Standards in Bury
SPD6	Supplementary Planning Document 6: Alterations & Extensions

Issues and Analysis

The following report includes analysis of the merits of the application against the relevant policies of both the National Planning Policy Framework (NPPF) and the adopted Bury Unitary Development Plan (UDP) together with other relevant material planning considerations. The policies of the UDP that have been used to assess this application are considered to be in accordance with the NPPF and as such are material planning considerations. For simplicity, just the UDP Policy will be referred to in the report, unless there is a particular matter to highlight arising from the NPPF where it would otherwise be specifically mentioned.

Housing Principle - Following revocation of the North West Regional Strategy on 20th May 2013, there is no statutory housing target for Bury. Work has commenced on the Greater Manchester Spatial Framework and this will bring forward a new statutory housing target for the Borough. This will subsequently be incorporated into Bury's future Local Plan.

In the meantime, the National Planning Policy Framework should be treated as a material planning consideration and it emphasises the need for local planning authorities to boost the supply of housing to meet local housing targets in both the short and long term. There is a particular emphasis, as in previous national planning guidance, to identify a rolling five year supply of deliverable land.

UDP Policy H1/2 states that the Council will have regard to various factors when assessing a proposal for residential development, including whether the proposal is within the urban

area, the availability of infrastructure and the suitability of the site, with regard to amenity, the nature of the local environment and the surrounding land uses.

UDP Policies H2/1 - The Form of New Residential Development and H2/2 - The Layout of New Residential Development takes into consideration factors relating to the height and roof type of adjacent buildings, the impact of developments on residential amenity, the density and character of the surrounding area and the position and proximity of neighbouring properties. Regard is also given to parking provision and access, landscaping and protection of trees/hedgerows and external areas.

UDP Policy H2/6 - Garden and Backland Development assesses proposals with special regard given to the concentration and relative density of such development in the surrounding area, impact on neighbouring properties and the local environment and access arrangements.

Supplementary Planning Document 6 - Alterations and Extensions to Residential Properties provides useful guidance in terms of acceptable aspect standards between dwellings and design criteria.

The development would be located within an established residential cul de sac and would therefore not conflict with the local environment in terms of character and land use. The proposal demonstrates that the existing access which leads to the site could be utilised without requiring significant modifications. There is existing infrastructure in place to facilitate two dwellings and the scale of the proposal would be such that there would not be over-development of the site.

The principle is therefore considered to be acceptable and would comply with UDP Policies H1/2, H2/1, H2/2 and H2/6 and the principles of the NPPF.

Policy principle - The proposal is partly within the River Valley (UDP Policy OL5/2) and Wildlife Corridor (UDP Policy EN6/4).

Under Policy OL5/2 - Development in River Valleys - New buildings or changes of use of land will not be permitted and the only exceptions that would be considered acceptable would be where development would not lead to the division of the open parts of the land into sections and where it would meet one of five circumstances, one of which relates to limited infilling in an established settlement. In principle, and subject to detail, when taking account of the small part of the river valley that would be affected, it is considered the development would not divide the valley into sections or obstruct access through the valley. It is also considered that the proposal would potentially meet the criterion related to limited infilling in an established settlement.

Policy EN6/4 - Wildlife Links and Corridors - states that the Council will not permit development which would adversely affect these areas and will seek to ensure that development within or adjacent these areas contribute to their effectiveness through design, landscaping and siting and mitigation works where appropriate. Again, only a small part of the wildlife corridor would be affected. In principle and subject to detail, the proposal has the potential to meet this policy provided that the necessary mitigation is provided.

The proposal shares the boundary with Roch Valley Greenway which is Protected Recreation under policy RT1/1. The site in question does not fulfil a recreational function and as such it is considered that the proposal would not conflict with this policy or cause any detrimental effects to this land.

As such, it is considered that the proposed development would not have a detrimental impact on the River Valley or Wildlife Corridor and that a suitably worded condition would ensure that appropriate boundary treatment and mitigation measures would be provided.

Access and parking - This has been applied for as a detail of the outline application. The

cul de sac is unadopted but well maintained and tarmaced. It is only a short stretch of road and whilst there is a fairly sharp bend into the cul de sac, the road adequately serves the 5 houses which are currently located here.

The application proposes to provide a passing place which would be located on the right hand side of the bend as the site is entered. This would both improve visibility into and out of the site and reduce conflict between car users and users of the footpath that emerges onto Meadway at this point. This is considered to bring about a benefit not only for new occupiers of the development but those residents who currently reside there.

The plans show that driveways would be located in front of the properties and provide for at least 2 parking spaces as well as a double garage. This is purely indicative at this stage. Were the proposed two dwellings to be 4/5 bed (similar to those surrounding), the parking provision which has been indicatively shown, is considered to be acceptable.

It is considered that the development for 2 additional houses would not significantly add to the volume of traffic to this area to any extent to cause highway safety concerns, and the works proposed to the site entrance would benefit the existing access arrangements to the site.

Subject to conditions, the Highway's section have raised no objection to the application and as such, the proposal is considered to be acceptable and would comply with UDP Policies H2/2 - The Layout of New Residential development, H2/6 - Garden and Backland Development, EN1/2 - Townscape and Built Design, HT2/4 - Car Parking and New Development and HT6/2 - Pedestrian/Vehicular Conflict.

Public Right of Way - There is a public footpath next to the access to the site which follows the River Roch. The proposed development would not encroach onto the footpath, but the Applicant should be aware that should the Right of Way be affected, an appropriate closure or diversion order would be required.

Layout and siting - Whilst the application has been submitted in outline with details of access only, an indicative layout plan has been submitted which shows that the two proposed dwellings would be of not too dissimilar footprint to the other houses on this cul de sac, and could be accommodated within the proposed site area.

The development also indicates the capability of providing a double garage and driveway to each plot, if an application were to come forward in this form, which would provide sufficient levels of parking. Garden areas would be located to the rear and side of the dwellings, provision of which is considered acceptable.

In terms of siting and the relationship of the proposed dwellings to the existing properties on Meadway, the layout indicates that separation distances and aspect standards would be achievable and would be compliant with policy guidance. Access to each of the existing properties would not be affected by the development and as discussed in the section above, it is considered that 2 additional properties within the cul de sac would not cause significant access issues or justify a reason for refusal of the application.

As such, it is considered that the indicative layout could provide a development that would not adversely impact on neighbouring properties and the local environment and would be acceptable and in compliance with UDP Policies H2/1 - The Form of New Residential development, H2/2 - The Layout of New Residential Development and H2/6 - Garden and Backland Development.

Flooding - The majority of the site is in Flood Zone 1 and as the site would be less than one hectare in size, a Flood Risk Assessment (FRA) is not required. The EA have been consulted and as there would be no built development in Flood Zone 2, the EA do not have any formal comments to make, and have referred the application to the Local Planning Authority to advise the applicant accordingly.

As the application is submitted in outline only, with details of access only, a drainage scheme at this stage would not be needed at this stage. However, it would be required in the submission of a Reserved matters application and as such a condition would be included to this effect, should outline permission be granted.

The residents have made reference to the sewer pipe which runs through the site and the requirement for an easement either side of this pipe. Should this result in the built development being pushed further towards the River, and into Flood Zone 2, then a FRA would be required and would be evidenced at a Reserved Matters stage in the layout.

As such, and on the basis of the application as submitted, the proposals are considered to be acceptable and would comply with chapter 10 - meeting the challenge of climate change, flooding and coastal change of the NPPF and UDP Policy EN5/1 - New Development and Flood Risk.

An informative to the applicant to advise of flood resistant measures would be included.

Trees - The application proposes the removal of two trees along the eastern boundary. The trees are not protected. There are a number of trees on this boundary which would be retained and it is considered with an appropriate landscaping scheme, the removal of two trees would not be seriously detrimental to the Wildlife Corridor or the Roch Valley Greenway.

The trees are not protected and the ecological survey which has been carried out of the site, identifies that there is no vegetation of significance, comprising mainly self seeded, relatively young trees and shrubbery, and as such would not be worthy of a Tree Preservation order. However, there would be a decline in site biodiversity due to the loss of semi natural vegetation, which can be compensated by a landscaping scheme which would be included as a condition.

As such, the proposal is considered to be acceptable and would comply with Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design, EN6/4 - Wildlife Links and Corridor and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

Ecology - An Ecological Impact Assessment has been submitted with the application and GMEU have been consulted. No significant constraints were identified by the developer's ecological consultant and it is advised by GMEU that ecological issues relating to invasive species and nesting birds could be resolved by condition.

Invasive species - Japanese knotweed was identified adjacent to the development site. The roots of this species can extend up to 7m from the stems and therefore earthworks on the site could spread this species which can propagate from root fragments. Japanese knotweed is included within schedule 9 part 2 of the Wildlife and Countryside Act 1981, as amended. It is an offence to introduce or cause to grow wild any plant listed under this schedule. A condition is therefore recommended that a method statement be submitted detailing eradication, control or avoidance methods be submitted for approval.

Nesting birds - It is not clear from the application whether the development would result in the clearance of any of the trees along the eastern boundary. These were assessed as providing some bird nesting potential. All birds nests and eggs (with certain limited exception) are protected by Section 1 of the Wildlife and Countryside Act 1981, as amended. As a precaution, a condition is recommended that works to vegetation be restricted between March and August unless a bird nest survey is carried out and submitted for approval.

Coal Authority - The application site falls within the defined Development High Risk Area and records indicate that the site is in an area of likely historic unrecorded underground coal

mining workings at shallow depth.

The Coal Mining Assessment submitted with the application concludes that there would be a potential risk posed to the development from past coal mining activity, and recommends that intrusive site investigations be carried out in respect of the coal mining legacy issues on the site. The report also recommends that gas protection measures are installed within the development. The findings of the intrusive site investigations should inform any remedial measures which may be required.

The Coal Authority have raised no objection and recommend a condition requiring the site investigation works prior to commencement of development, together with any remedial works also identified.

Response to objectors -

- Disruption from construction traffic is not a material planning consideration.
- Japanese knotweed - A condition recommended by GMEU has been included which requires the submission of a method statement detailing eradication/control/avoidance measures for Japanese knotweed be submitted for approval.
- Road into cul de sac - A passing place would be provided which would improve access in/out the site and the Traffic Section are satisfied with the specification provided. ownership issues and maintenance of hedges/trees are not material planning considerations and private matters between the Applicant and residents.
- Land ownership - The correct certification of ownership has been signed by the Applicant. Any development to be carried out on land not in the Applicant's control or ownership is a private matter between respective landowners.
- Flood plain - The site is located in Flood Zone 2 with a very small area to the south of the site located in Flood Zone 2. The EA have been consulted but have no comments to make.
- Main sewer and electric cables - UU have been consulted and raise no objection to the proposal in principle, but would not allow built development over or within 3m of the centreline of the sewer. The proposed plan demonstrates that the dwellings would be approximately 4m from the centreline, which the layout at Reserved matters stage would confirm. The response from UU has been passed to the Applicant who are aware of the restrictions and measures which may be necessary to facilitate the development. This is a matter for the Applicant to address.
- Cramming - The indicative layout demonstrates that there would be acceptable separation distances between the proposed and existing dwellings. The plans show that acceptable levels of outdoor amenity areas could be achieved. As the application is outline and design and appearance not applied for at this stage, there are no elevation plans submitted to assess design or appearance of the proposed dwellings. It is considered that two additional dwellings in this location would not significantly increase vehicular noise and pollution from cars.
- Wildlife - Issues addressed regarding ownership of trees and their removal is a private matter. A landscaping scheme would be required by condition.
- Coal Authority - Recommend a condition that details of intrusive site investigations and remedial measures are submitted for approval.

Statement in accordance with Article 35(2) Town and Country Planning (Development Management Procedure) (England) (Amendment) Order 2015

The Local Planning Authority worked positively and proactively with the applicant to identify various solutions during the application process to ensure that the proposal comprised sustainable development and would improve the economic, social and environmental conditions of the area and would accord with the development plan. These were incorporated into the scheme and/or have been secured by planning condition. The Local Planning Authority has therefore implemented the requirement in Paragraphs 186-187 of the National Planning Policy Framework.

Recommendation: Approve with Conditions

Conditions/ Reasons

1. Applications for approval of reserved matters must be made not later than:
 - the expiration of three years beginning with the date of the grant of outline planning permission; and
 - that the development to which the permission relates must be begun not later than the expiration of two years from the final approval of the reserved matters or, in the case of approval on different dates, the final approval of the last such matter to be approved.

Reason. Required to be imposed pursuant to Section 51 of the Planning and Compulsory Purchase Act 2004

2. Before the development is commenced, the applicant shall submit detailed plans and particulars to the Local Planning Authority, and obtain their approval under the Town and Country Planning Acts, of the following reserved matters; the layout, scale, appearance, and the landscaping of the site.

Reason. To ensure the satisfactory development of the site and because this application is in outline only.

3. No development shall commence unless and until:-
 - A contaminated land Preliminary Risk Assessment report to assess the actual/potential contamination and/or ground gas/landfill gas risks at the site shall be submitted to, and approved in writing by, the Local Planning Authority;
 - Where actual/potential contamination and/or ground gas/landfill gas risks have been identified, detailed site investigation and suitable risk assessment shall be submitted to, and approved in writing by the Local Planning Authority;
 - Where remediation/protection measures is/are required, a detailed Remediation Strategy shall be submitted to, and approved in writing by, the Local Planning Authority.

Reason. The scheme does not provide full details of the actual contamination and subsequent remediation, which is required to secure the satisfactory development of the site in terms of human health, controlled waters, ground gas and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

4. Following the provisions of Condition 3 of this planning permission, where remediation is required, the approved Remediation Strategy must be carried out to the satisfaction of the Local Planning Authority within agreed timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the development being brought into use.

Reason. To secure the satisfactory development of the site in terms of human health, controlled waters and the wider environment and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

5. Following the provisions of Condition 3 of this planning permission, where ground gas remediation / protection measures are required, the approved Remediation Strategy must be carried out to the written satisfaction of the Local Planning Authority within approved timescales; and A Site Verification Report detailing the actions taken and conclusions at each stage of the remediation works, including substantiating evidence, shall be submitted to and approved in writing by the Local Planning Authority prior to the

development being brought into use.

Reason. To alleviate any possible risk associated with the production of landfill gas and ground gas in accordance with the recommendations of the Environment Agency and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.

6. Prior to any earthworks a method statement detailing eradication and/or control and/or avoidance measures for Japanese knotweed should be submitted to and approved by the Local Planning Authority. The approved method statement shall include a timetable for implementation. Should a delay of more than one year occur between the date of approval of the management scheme and either the date of implementation of the management scheme or the date of development commencing, a further site survey must be undertaken and submitted to the Local Planning Authority. The approved method statement only shall be adhered to and implemented in full.
Reason. The scheme does not provide full details of the actual extent of Japanese Knotweed and Himalayan Balsam in the interest of UDP Policy EN9 - Landscape and pursuant to National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
7. No works to trees or shrubs shall occur between the 1st March and 31st August in any year unless a detailed bird nest survey by a suitably experienced ecologist has been carried out immediately prior to clearance and written confirmation provided that no active bird nests are present which has been agreed in writing by the Local Planning Authority.
Reason. In order to ensure that no harm is caused to a Protected Species pursuant to policies EN6 – Conservation of the Natural Environment and EN6/3 – Features of Ecological Value of the Bury Unitary Development Plan and National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
8. All trees to be retained on site shall be protected in accordance with BS 5837:2012 "Trees in relation to design, demolition and construction". The development shall not commence unless and until the measures required by the British Standard are implemented and all measures required shall remain in situ until the development has been completed.
Reason. To avoid the loss of trees which are of amenity value to the area pursuant to Policy EN1/2 - Townscape and Built Design and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan and National Planning Policy Framework Section 11 - Conserving and enhancing the natural environment.
9. Full details of the proposed passing place, driveway positions and parking arrangements shown indicatively on approved plan reference 16-047 1100 Revision F, along with appropriate visibility splays in accordance with the standards in Manual for Streets and refuse collection/bin storage arrangements agreed with Waste Management & Transport, shall be submitted at first reserved matters application stage. The details subsequently approved shall be implemented to the written satisfaction of the Local Planning Authority prior to the dwellings hereby approved being first occupied and thereafter maintained.
Reason. To ensure good highway design and the intervisibility of the users of the site and the adjacent shared access in the interests of highway safety pursuant to Bury Unitary Development Plan Policies H2/2 - The layout of New Residential Development, H2/6 - Garden and Backland Development, EN1/2 - Townscape and Built design and HT2/4 - Car Parking and New Development.
10. No development shall commence unless and until a 'Construction Traffic Management Plan' (CTMP), has been submitted to and agreed in writing with the

Local Planning Authority and shall confirm/provide the following:

- Hours of operation and number of vehicle movements;
- Arrangements for the turning and manoeuvring of vehicles within the curtilage of the site;
- Parking on site or on land within the applicant's control of operatives' and construction vehicles together with storage on site of construction materials.

The approved plan shall be adhered to throughout the construction period and the measures shall be retained and facilities used for the intended purpose for the duration of the construction period. The areas identified shall not be used for any other purposes other than the turning/parking of vehicles and storage of construction materials.

Reason. Insufficient information submitted at application stage. To mitigate the impact of the construction traffic generated by the proposed development on the adjacent residential streets and ensure adequate off street car parking provision and materials storage arrangements for the duration of the construction period, in the interests of highway safety pursuant to Bury Unitary Development Plan Policy EN1/2 - Townscape and Built Design.

11. No development shall commence unless and until details have been submitted to and approved by the Local Planning Authority to cover measures to ensure that all mud and other loose materials are not carried on the wheels and chassis of any vehicles leaving the site and measures to minimise dust nuisance caused by the operations. The approved details shall be implemented and maintained thereafter during the period of demolition/construction.

Reason. Insufficient information submitted at application stage. To ensure that the adopted highways are kept free of deposited material from the ground works operations pursuant to Bury Unitary Development Plan Policy EN1/2 - Townscape and Built Design.

12. A minimum hardstanding of 5.5m measured between the shared access and any proposed garage doors shall be provided to the written satisfaction of the Local Planning Authority and thereafter maintained.

Reason. To enable a vehicle to stand clear of the highway whilst the garage doors are opened and to allow adequate space to maintain a vehicle clear of the highway in the interests of road safety pursuant to Policies H2/2 - The layout of New Residential Development, H2/6 - Garden and Backland Development and HT2/4 - Car parking and New Development of the Bury Unitary Development Plan.

13. At Reserved Matters stage, details of boundary treatment and landscaping shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include measures to mitigate for any loss of habitat or ecological value of the Wildlife Corridor which would be affected by the siting and layout of the proposed development. The approved scheme only shall be implemented within the first available planting season following substantial completion of the development. Any trees or shrubs removed, dying or becoming severely damaged or becoming severely diseased within five years of planting shall be replaced by trees or shrubs of a similar size or species to those originally required to be planted to the written satisfaction of the Local Planning Authority.

Reason. To secure the satisfactory development of the site and in the interests of visual amenity pursuant to Policies H2/2 - The Layout of New Residential Development, EN1/2 - Townscape and Built Design, EN6/4 - Wildlife Links and Corridor and EN8/2 – Woodland and Tree Planting of the Bury Unitary Development Plan.

14. This decision relates to drawings numbered 16-047 - 1000 rev D/1001 Rev D/1100 Rev F and the development shall not be carried out except in accordance with the drawings hereby approved.

Reason. For the avoidance of doubt and to ensure a satisfactory standard of design pursuant to the policies of the Bury Unitary Development Plan listed.

15. No development shall commence unless and until details of surface water drainage proposals have been submitted to and approved by the Local Planning Authority. The scheme should be in accordance with the submitted Surface Water Sustainable Drainage Assessment and must be based on the hierarchy of drainage options in the National Planning Practice Guidance and be designed in accordance with the Non-Statutory Technical Standards for Sustainable Drainage Systems (March 2015). This must include assessment of potential SuDS options for surface water drainage with appropriate calculations and test results to support the chosen solution. Details of proposed maintenance arrangements should also be provided. The approved scheme only shall be implemented and thereafter maintained.

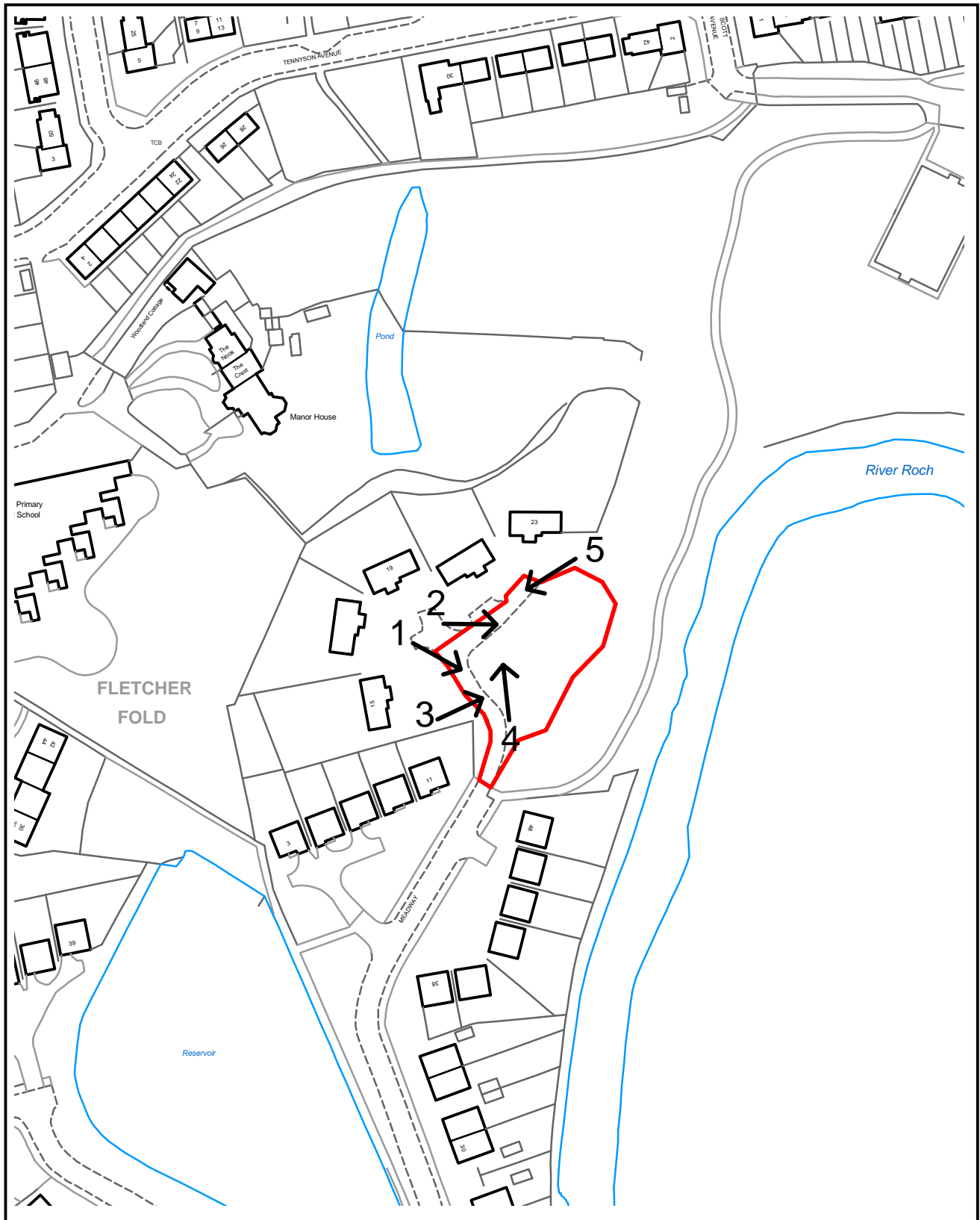
Reason. To promote sustainable development. The current application contains insufficient information regarding the proposed drainage scheme to fully assess the impact pursuant to chapter 10 - Meeting the challenge of climate change, flooding and coastal change of the NPPF.

16. No development shall commence unless and until:
- A scheme of intrusive site investigations has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include details of the undertaking of that scheme together with a timetable of the works;
 - The submission of a report of findings arising from the intrusive site investigations;
 - Where remediation works/measures are required, a detailed scheme of remedial works and implementation of those remedial works shall be submitted to, and approved in writing by the Local Planning Authority.
 - The works shall thereafter be carried in accordance with the approved scheme and details.

Reason. Information has not been submitted at application stage, which is required to ensure the safety and stability of the proposed development and to alleviate possible risk pursuant to chapter 11 - Conserving and enhancing the natural environment.

For further information on the application please contact **Jennie Townsend** on **0161 253-5320**

Viewpoints



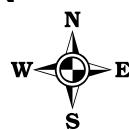
PLANNING APPLICATION LOCATION PLAN

APP. NO 61369

ADDRESS: Land adj to 23 Meadway
Bury

Planning, Environmental and Regulatory Services

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Bury
COUNCIL

61369

Photo 1



Photo 2



Photo 3



Photo 4

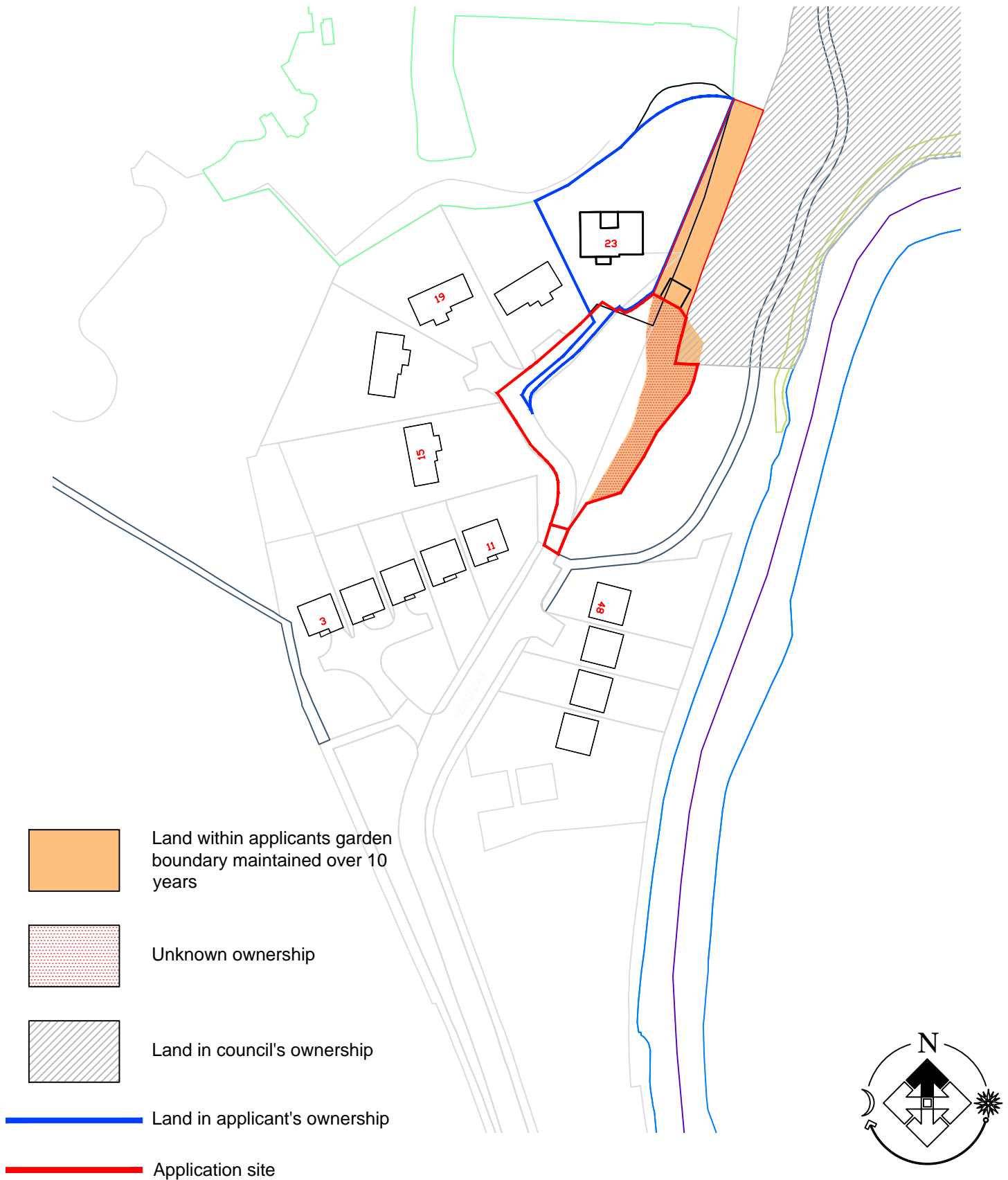


Photo 5

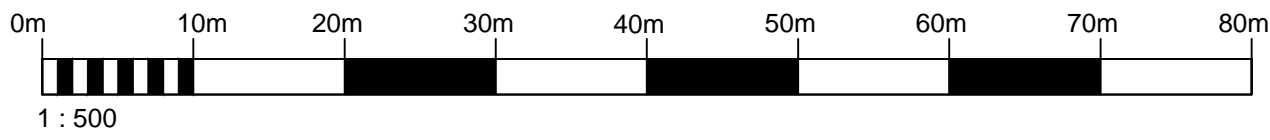




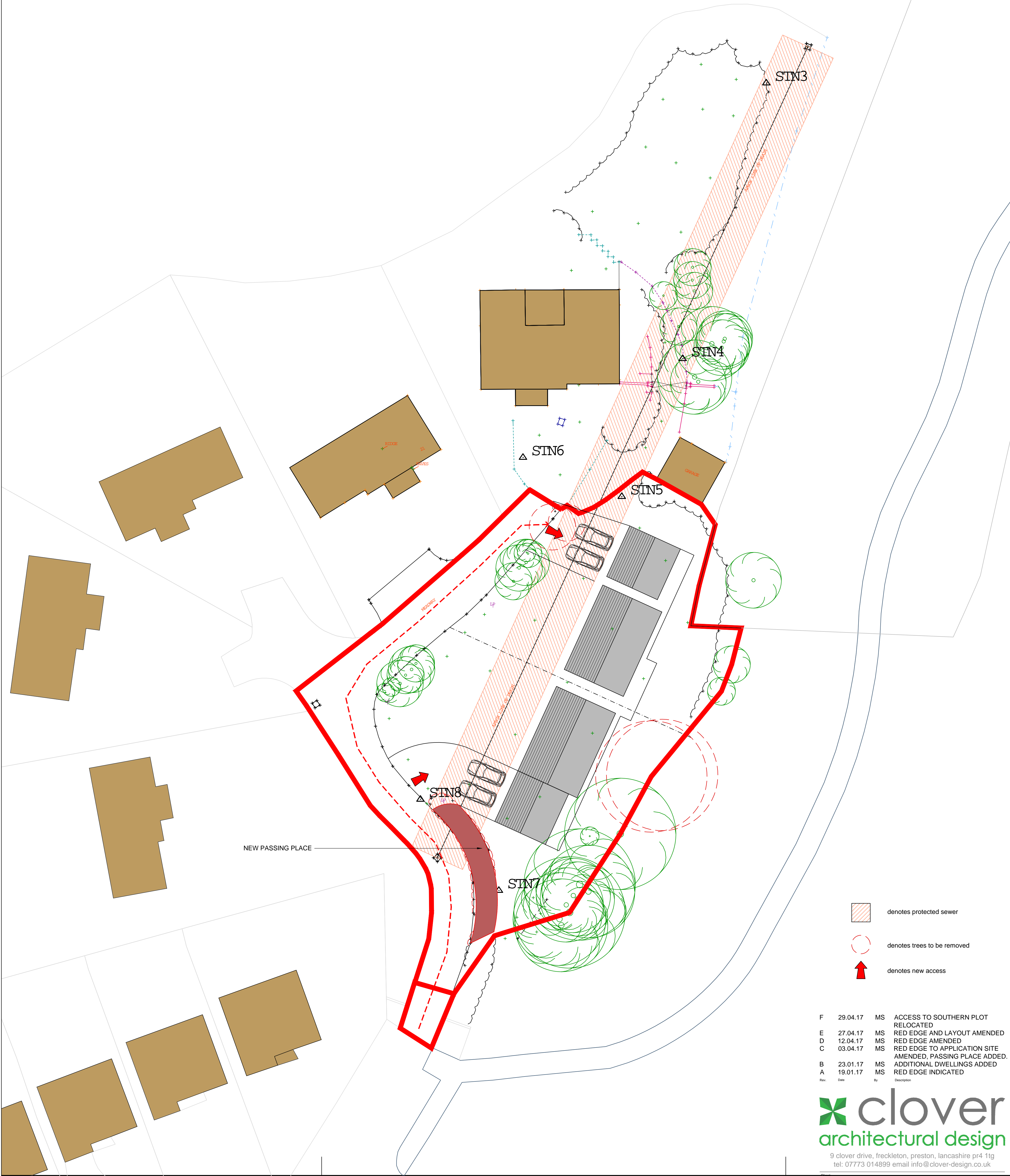
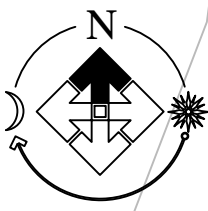
Do not scale from this drawing
2: All dimensions must be checked on site prior to commencement of work
3: For details of all structural, mechanical and electrical work, refer to relevant specialist drawing.






client:	Mr Graham Philips	scale:	1:1250	date:	Sep 16	drawn:	MS
project:	Proposed Development on Land Adj 23 Meadway, Bury	project no:	16-047	drawing no:	1000	rev:	D
title:	Location Plan (OS Plan overlaid with deed plan)						



general notes:
do not scale the drawing.
all dimensions to be checked on site prior to commencement of work and any
discrepancy shall be immediately reported and resolved prior to work commencing.
this drawing is to be read in conjunction with all relevant drawings and
specifications relating to the job whether or not indicated on the drawing.
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-  denotes protected sewer
-  denotes trees to be removed
-  denotes new access

F	29.04.17	MS	ACCESS TO SOUTHERN PLOT RELOCATED
E	27.04.17	MS	RED EDGE AND LAYOUT AMENDED
D	12.04.17	MS	RED EDGE AMENDED
C	03.04.17	MS	RED EDGE TO APPLICATION SITE AMENDED, PASSING PLACE ADDED.
B	23.01.17	MS	ADDITIONAL DWELLINGS ADDED
A	19.01.17	MS	RED EDGE INDICATED
Rev.	Date	By	Description

 **clover**
architectural design

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Mr Graham Philips
23 Meadoway
Bury

Project:
Proposed development
Land adj 23 Meadoway
Bury

Drawing Title:
Proposed Site Layout

Drawn: ms	Checked:	Scale: 1:500	Date: Jan 17
Job No: 16-047	Drawing No: 1100	Rev: F	

PROPOSED SITE LAYOUT

A1

PLANNING

Plot: 23meadway, freckleton, preston, lancashire pr4 1tg. Project: 16-047. Drawing: 1100. Date: 16-01-17. Rev: F.